

ORDINANCE NUMBER 18-O-023**AN ORDINANCE TO REZONE AND AMEND THE OFFICIAL ZONING MAP OF THE CITY OF SUFFOLK, TO CHANGE THE ZONING FROM CP, COMMERCE PARK ZONING DISTRICT, TO MUD, MIXED USE DEVELOPMENT OVERLAY ZONING DISTRICT FOR PROPERTY LOCATED AT 6500 COLLEGE DRIVE, ZONING MAP 6, PARCEL 35; RZ15-16 (CONDITIONAL)**

WHEREAS, David Gianascoli, agent for Gee's Partners, LLC, applicant, on the behalf of the Economic Development Authority of the City of Suffolk, property owner, has requested the rezoning for certain tracts of land situated in the City of Suffolk, Virginia, as Zoning Map 6, Parcel 35, which land is depicted on Exhibit "C"; and

WHEREAS, the proposed rezoning and amendment to the official zoning map have been advertised and reviewed by the Planning Commission in compliance with the requirements of state law; and

WHEREAS, the Planning Commission has made a recommendation as stated in Exhibit "A"; and

WHEREAS, a public hearing before City Council was duly advertised as required by law and held on the 21st day of March, 2018, at which public hearing the public was presented with the opportunity to comment on the proposed rezoning.

NOW, THEREFORE, BE IT ORDAINED by the Council of the City of Suffolk, Virginia, that:

Section 1. Exhibit "A", "Planning Commission Recommendation ", Exhibit "B", "Rezoning Proffers", Exhibit "C", "Property Map", Exhibit "D", "Zoning Exhibit", and Exhibit "E", "Design Guidelines" which are attached hereto, are hereby incorporated as part of this ordinance.

Section 2. Findings.

A. Council finds that the proposed rezoning is reasonable, and warranted due to a mistake or change in circumstances affecting the property; and has considered the following factors and finds that the proposed rezoning does not conflict with:

1. the existing use and character of property within the City;
2. the Comprehensive Plan;
3. the suitability of the property for various uses;
4. the trends of growth or change;

5. the current or future requirements of the community as to land for various purposes as determined by population and economic studies and other studies;
6. the transportation requirements of the community;
7. the requirements for airports, housing, schools, parks, playgrounds, recreation areas and other public services;
8. the conservation of natural resources;
9. the preservation of flood plains;
10. the preservation of agricultural and forestal land;
11. the conservation of properties and their values;
12. the encouragement of the most appropriate use of land throughout the City; and,
13. the expressed purpose of the City's Unified Development Ordinance (UDO) as set out in Section 31-102 of the UDO, as amended, and Section 15.2-2283 of the Code of Virginia, (1950), as amended.

B. The Suffolk City Council makes the following specific findings as to the conditions of the rezoning:

1. the proposed rezoning itself gives rise to the need for the proffered conditions;
- 4.
2. the proffered conditions have a reasonable relationship to the rezoning; and
3. the proffered conditions are in conformity with the 2035 Comprehensive Plan as adopted by City Council on April 1, 2015.

Section 3. Rezoning.

1. The conditions proffered in the attached Exhibit "B" be, and they are hereby, accepted.
2. The property as shown on the attached Exhibit "C" is hereby, conditionally rezoned and the official zoning map be, and it is hereby, amended from CP, Commerce Park Zoning District, to MUD, Mixed Use Development Overlay Zoning District.

Section 4. Recordation.

A certified copy of this ordinance shall be recorded, by the applicant, in the name of the property owner as grantor in the office of the Clerk of the Circuit Court of the City of Suffolk, Virginia.

This ordinance shall be effective upon passage and shall not be published or codified.

READ AND PASSED: MARCH 21, 2018

TESTE: Erika S. Dawley
Erika S. Dawley, City Clerk

RESOLUTION NO. 18-02-1

**CITY OF SUFFOLK PLANNING COMMISSION
A RESOLUTION TO PRESENT A REPORT AND RECOMMENDATION
TO CITY COUNCIL RELATING TO REZONING REQUEST
RZ15-16 (CONDITIONAL)
TO CHANGE THE ZONING FROM CP, COMMERCE PARK, TO MUD, MIXED USE
DEVELOPMENT DISTRICT, FOR PROPERTY LOCATED AT 6500 COLLEGE
DRIVE, ZONING MAP 6, PARCEL 35**

WHEREAS, David Gianascoli, agent for Gee's Partners, LLC, applicant, on the behalf of the Economic Development Authority of the City of Suffolk, property owner, has requested a change in zoning from CP, Commerce Park zoning district, to MUD, Mixed Use Development Overlay District (Conditional); and

WHEREAS, the procedural requirements for the consideration of this request by the Planning Commission have been met.

NOW, THEREFORE, BE IT RESOLVED by the Planning Commission of the City of Suffolk, Virginia, that:

Section 1. Findings.

- A. The Suffolk Planning Commission finds that the proposed rezoning is reasonable, and warranted due to a change in circumstances affecting the property, and has considered the following factors and finds that the proposed rezoning does not conflict with:
1. the existing use and character of property within the City;
 2. the Comprehensive Plan;
 3. the suitability of the property for various uses;
 4. the trends of growth or change;
 5. the current or future requirements of the community as to land for various purposes as determined by population and economic studies and other studies;
 6. the transportation requirements of the community;
 7. the requirements for airports, housing, schools, parks, playgrounds, recreation areas and other public services;
 8. the conservation of natural resources;
 9. the preservation of flood plains;
 10. the preservation of agricultural and forestal land;

- 11. the conservation of properties and their values;
- 12. the encouragement of the most appropriate use of land throughout the City;
- 13. the expressed purpose of the City's Unified Development Ordinances as set out in Section 31-102 of the Code of the City of Suffolk (1998), as amended, and Section 15.2-2283 of the Code of Virginia, (1950), as amended ("Va. Code").

B. The Suffolk Planning Commission makes the following specific findings to the conditions of the rezoning:

- 1. the proposed rezoning itself gives rise to the need for the proffered conditions;
- 2. the proffered conditions have a reasonable relationship to the rezoning; and
- 3. the proffered conditions are in conformity with the 2035 Comprehensive Plan as adopted by City Council on April 1, 2015.

Section 2. Recommendation to Council

The Planning Commission recommends to City Council that the request, RZ15-16 (Conditional), be:

- a. Granted as submitted, and the City Council adopt the proposed Ordinance without modification.
- b. Denied, and that Council not adopt the proposed Ordinance.
- c. Granted with the modifications set forth on the attached listing of specific recommendations, and that Council adopt the proposed Ordinance with such modifications.

READ AND PASSED: Feb 22 / 19
 TESTE: [Signature]

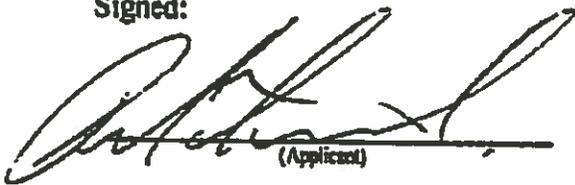
I hereby voluntarily proffer that the development of the property owned by me proposed for reclassification under this application shall be in strict accordance with the conditions set forth below.

The following conditions (add additional sheets if necessary) are voluntarily proffered for the reclassification of property identified as Tax Map 6*35, Block _____, Parcel Number _____:

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FEB 27 2018
PLANNING

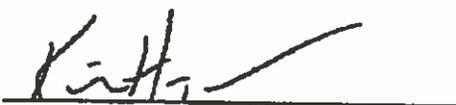
1. The Point at Harbour View Design Guidelines
2. Maximum 500 Residential Units
 - a. Maximum 300 for rent apartment units
 - i. The development(s) will feature a main entrance(s) providing for interior resident access
 - b. Maximum 100 for sale luxury townhomes/ condominiums
 - c. Maximum 100 for sale or for rent loft/mixed use building units
3. Maximum 476,000 square feet of commercial square footage
 - a. 300,000 square feet of retail
 - b. 100,000 square feet of office
 - c. 76,000 square feet of hotel
 - i. A 25% variation within the above listed retail, office & hotel square footage totals will be permissible to provide for market flexibility.
4. Elementary School: At the time of building permits; the developer will make a cash contribution to the City of Suffolk to advance capacity for additional students at the elementary school level. This contribution is calculated from the number of students generated, as reported in the Financial Impact Statement (dated February 5, 2018), at the elementary school level, multiplied by \$16,494.95 for each student. This contribution will be made upon issuance of the occupancy permit for each dwelling unit.
5. Middle School: At the time of building permits; the developer will make a cash contribution to the City of Suffolk to advance capacity for additional students at the middle school level. This contribution is calculated from the number of students generated, as reported in the Financial Impact Statement (dated February 5, 2018), at the middle school level, multiplied by \$31,945.06 for each student. This contribution will be made upon issuance of the occupancy permit for each dwelling unit.
6. High School: At the time of building permits; the developer will make a cash contribution to the City of Suffolk to advance capacity for additional students at the high school level. This contribution is calculated from the number of students generated, as reported in the Financial Impact Statement (dated February 5, 2018), at the high school level, multiplied by \$28,679.65 for each student. This contribution will be made upon issuance of the occupancy permit for each dwelling unit.

Signed:


(Applicant)

DAVID M. Giorgosco
(Printed Name)

12.14.17
(Date)



Kevin Hughes

12/14/17

EXHIBIT B

I hereby voluntarily proffer that the development of the property owned by me proposed for reclassification under this application shall be in strict accordance with the conditions set forth below.

The following conditions (add additional sheets if necessary) are voluntarily proffered for the reclassification of property identified as Tax Map (a # 35), Block _____, Parcel Number _____:

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MAR 13 2018
PLANNING

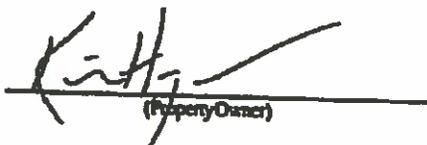
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Signed:


(Applicant)


DAVID M. Giorgis
(Principal Name)

3/13/2018
(Date)


(Property Owner)


Kevin Hughes
(Agent Name)

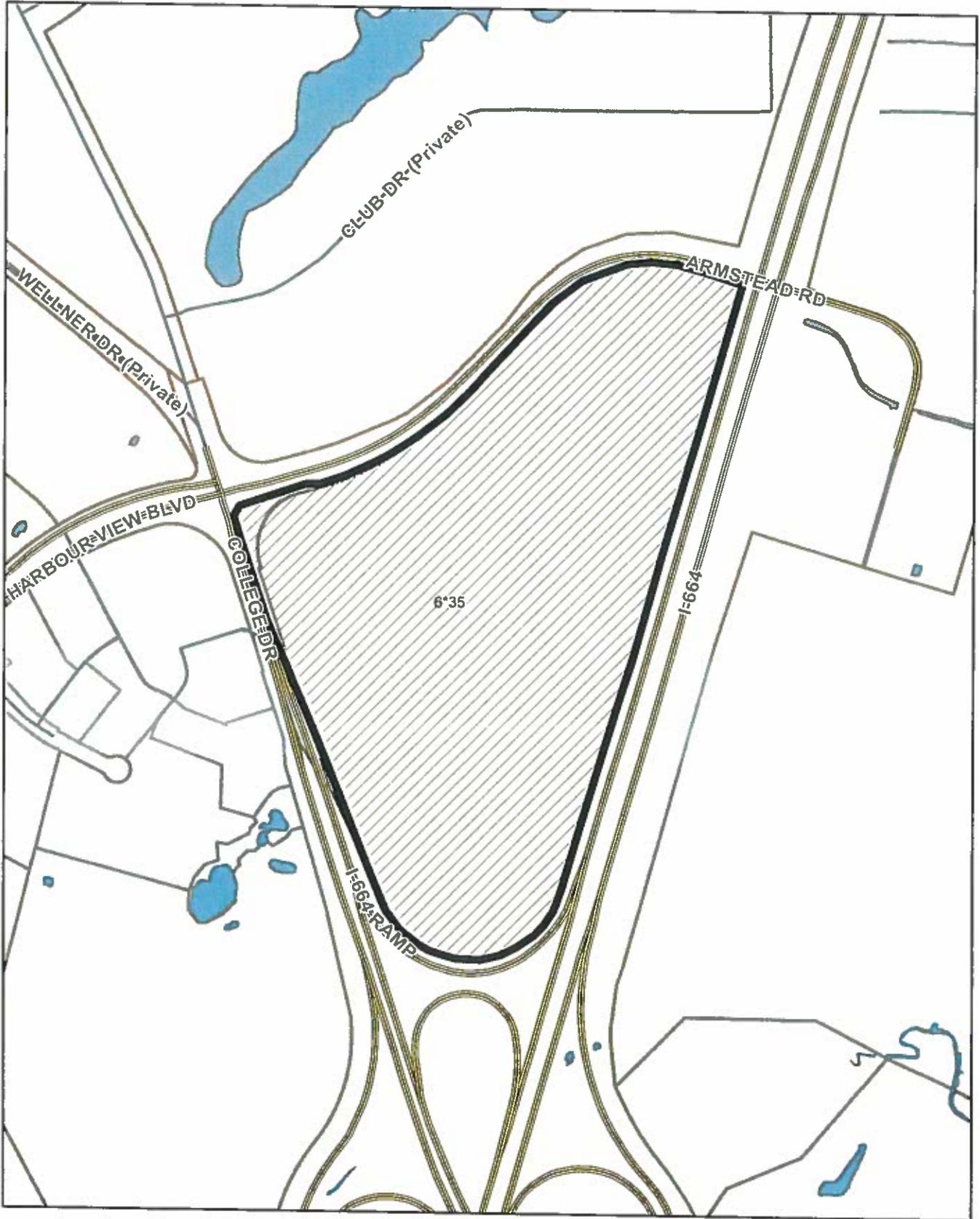
3/13/2018
(Date)

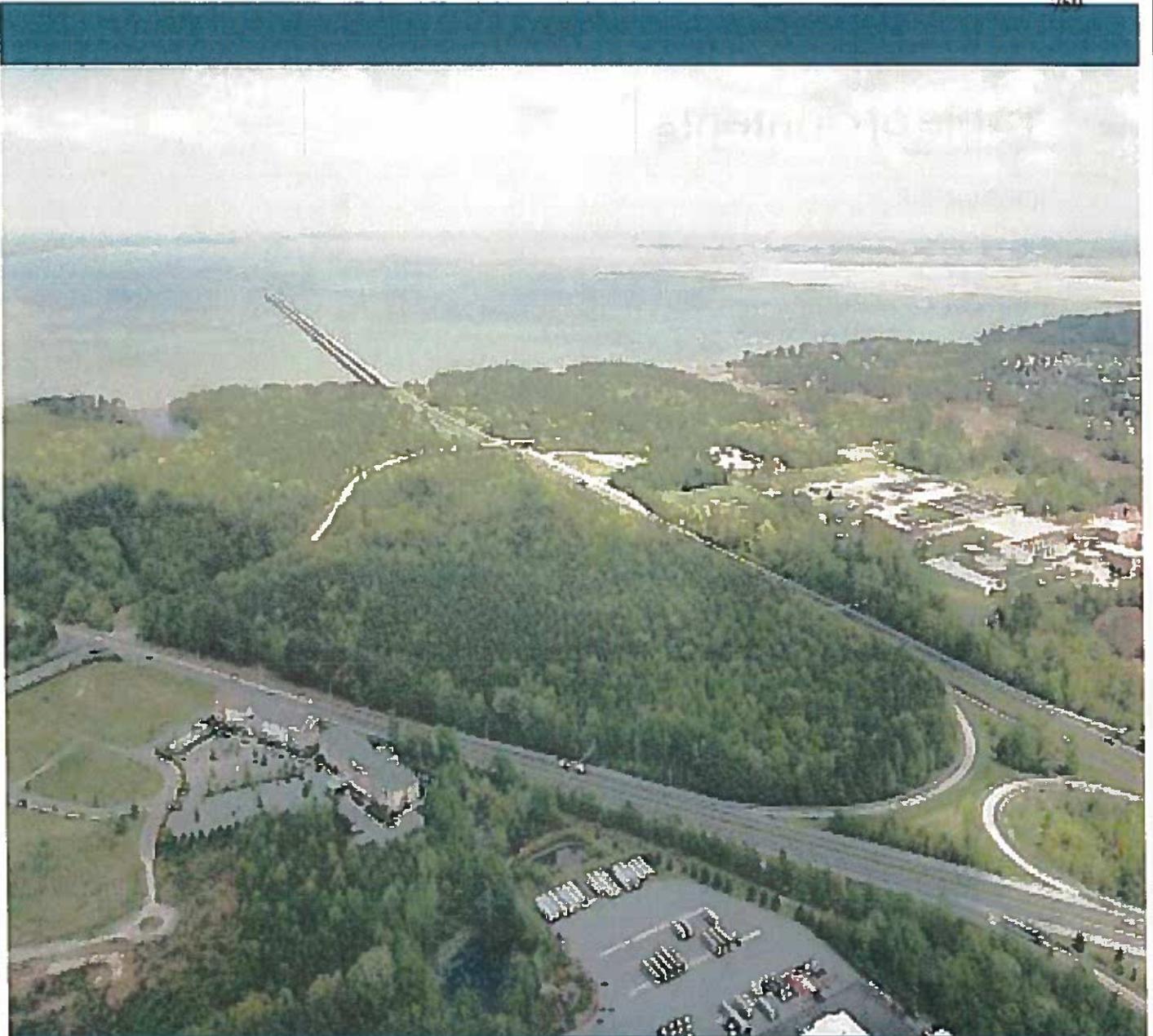


PROPERTY MAP RZ15-16

EXHIBIT C

757





The Point at Harbour View

Design Guidelines

Prepared for



Prepared by



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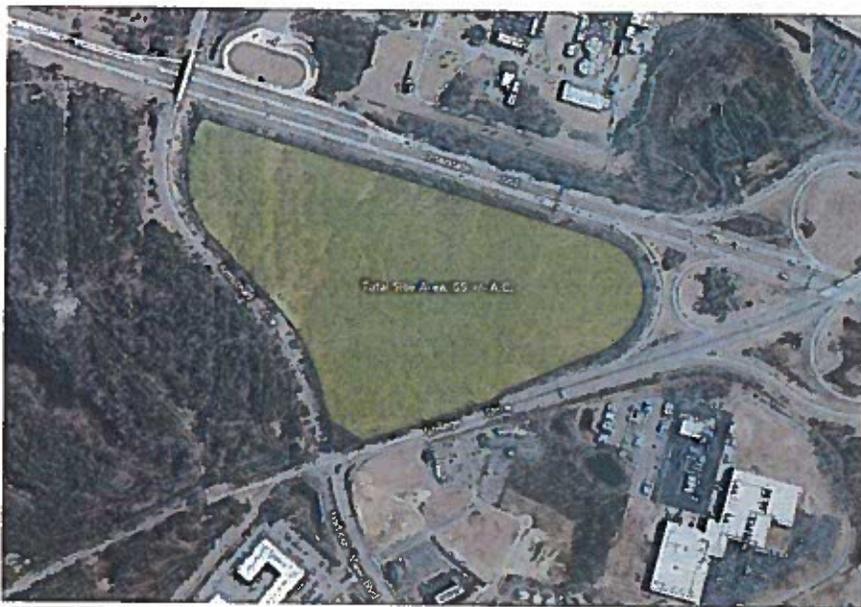
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February 12, 2018

Introduction

The Point at Harbour View is envisioned to be a combination of office, retail, and residential elements with the potential for institutional use. By virtue of these design guidelines, a development at The Point at Harbour View will attain a level of quality not necessarily afforded by conventional zoning standards. The designed proximity of residential and commercial components establishes a unique environment advantageous to those living, working, and visiting The Point at Harbour View. A variety of retail and other uses will provide an assortment of convenient services for the residents thereby creating a true walkable community that is closely positioned to retail and commercial services. In turn, jobs created on site can potentially be filled by those same residents further enhancing the quality of life through added health benefits, traffic relaxation, and reduced pollution. This site's location represents an opportunity to develop a pedestrian oriented augmentation of the City's Mixed Use Core district that will be supported by public transportation. Its location between Interstate 664 and College Drive creates a superior site for exceptional regional development. This improvement to the region will set the tone and reinvigorate the surrounding commercial area. It is these attributes and goals that make this plan an ideal candidate for Mixed Use Development zoning commensurate with Section 31-419 of the Unified Development Ordinance. This development will provide an ideal setting for the citizens of and visitors to the City of Suffolk.

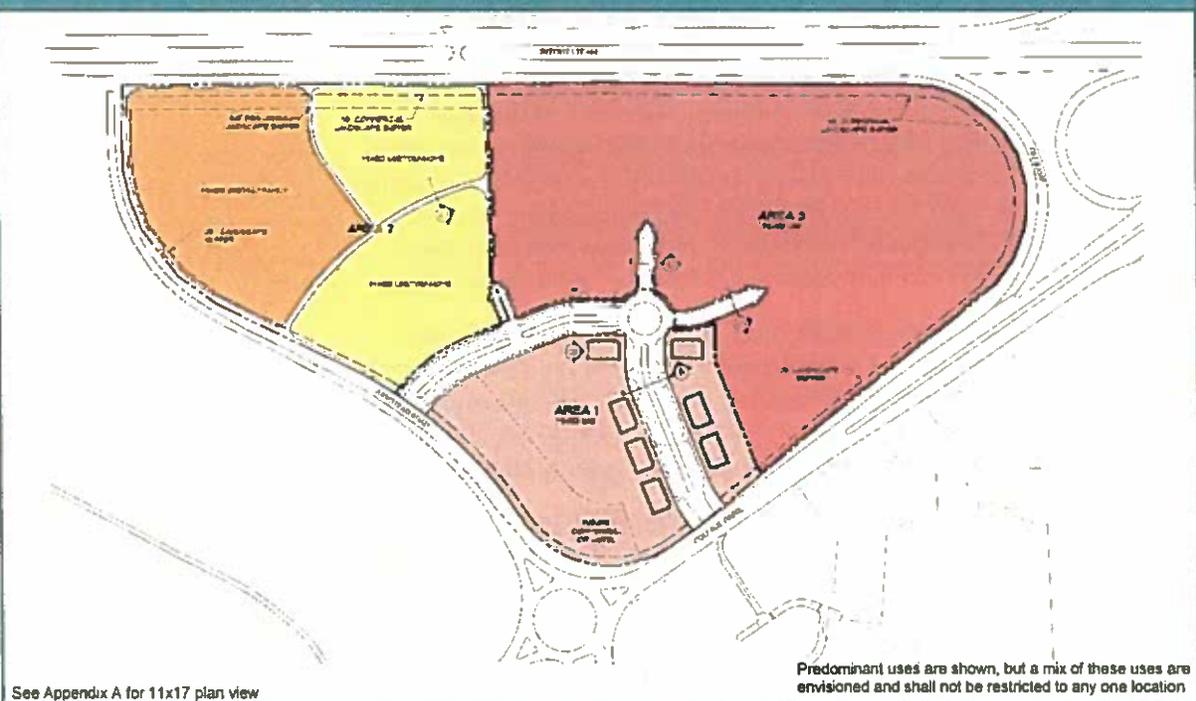


THE POINT AT HARBOUR VIEW



1.1 INTRODUCTION

The Point at Harbour View Major Areas



The project is viable because it draws from outside areas, rather than scavenging off of existing businesses. In this way, it supports the Comprehensive Plan for the Core by not degrading it but rather drawing people closer to it through development of the Mixed Use Core district. The Point at Harbour View creates a walkable project and can encourage walkability to nearby projects.

The site is attractive and well located in regard to access and proximity to employment, community services, shopping, medical, and transportation routes which provide extended access to points throughout the region. The location, rents, and amenity package appeal to moderate and higher income families and professionals. Recent residential growth throughout the City makes it a growing market for retail development as the retail development follows this growth in "rooftops." The retailers currently along College Drive and Harbour View Blvd. pull from a retail trade area that represents a population of more than 300,000 and draw business from not only Suffolk but the Peninsula and Isle of Wight County.

This project will enhance the development of employment centers in the Mixed Use Core district desired by the Comprehensive Plan. There is currently a large amount of traditional office space vacancy at the nearby Bridgeway offices. New concepts like Innovation Districts, as proposed at the Point at Harbour View, have the ability to bring new energy into suburban office developments. The Point at Harbour View development will give new opportunities for further growth in the office market in the Mixed Use Core district as again the population growth in the City fuels the need for new and expanded employment centers, especially in the medical and institutional categories that have been active and successful in recent years in the Harbour View Growth Area.

The Point at Harbour View is located less than a mile from the Monitor Merrimac Bridge Tunnel, and is sited in the Mixed Use Core district within the Northern Growth Area as described by the City's 2026 and proposed 2035 Comprehensive Plan. As seen in the existing site aerial, it is fifty-five acres framed by Interstate 664, College Drive, and Armistead Avenue.

The site is currently treed with little to no wetlands located on site.

GENERAL LAYOUT

The Point at Harbour View is divided into three main areas. Area 1 is located at the entrance to the development and consists of mixed use buildings that are visible from all sides. Area 1 infrastructure shall be installed first and will set the tone for future development (see image on page 2.1 Massing) Area two is between I-664 and Armstead Road and is mixed use with an emphasis on residential units. Area 3 takes up the remainder of the parcel and is home to mixed uses where anchor tenants and signature buildings are envisioned. Examples could include a signature hotel, multi-story office building, or a large anchor retail establishment.

The development will feature two main styles of development; residential and the other commercial. The specific layout and configuration of sites within those areas will be left to the site design process. For the purposes of preparing the required reports associated with the rezoning, assumptions were made about a conceptual plan of development. This is meant as a framework for analysis and is not to be interpreted as a prescriptive plan of development.

The site will be accessed by two primary egress-ingress points; one located off of College Drive will be right in and right out only. Another major access point will provide full access located on Armstead Road and will direct traffic to the new traffic circle at the corner of Harbour View Blvd. and College Drive. A secondary Armstead access, closer to I-664 may be needed, depending on site development.

Both the commercial and residential component will be of a high-quality nature. The building heights and the primary surface parking nature, land cover not dedicated to building footprint will be largely dedicated to vehicular storage and conveyance. Given this appropriation of available land it is critically important to articulate amenity and character within the interstitial spaces to convey an all-important sense of place. The following pages will be an expression and directive in support of the planning and construction that will deliver on this edict.

Open space offers a variety of benefits in the form of cultural preservation, stress management and increased property values on adjacent lands just to name a few. The City of Suffolk's Comprehensive Plan spells out the initiative to establish, enhance, and preserve open space. The Point at Harbour View development will provide opportunity for several open space configurations. Linear open spaces, trees saves, and gathering areas are anticipated. Office buildings may incorporate small park-like areas along their periphery. The land immediately adjacent to a stormwater pond as well as the interstitial spaces between residential buildings will afford open space specifically to The Point at Harbour View residents. Pathways and/or sidewalks will then lead to residential and potentially regional public open spaces. All will be accessible by multiple modes of travel depending on place type, including pedestrian, bike, public transit, or private vehicle. A system of sidewalks, crosswalks, and trails will connect The Point at Harbour View's open spaces with each other.

These design guidelines are organized in direct response to, and in the order enumerated by those topics prescribed by Section 31-419 (i) and (j) of the City of Suffolk's Unified Development Ordinance.

Massing

NON-RESIDENTIAL USES

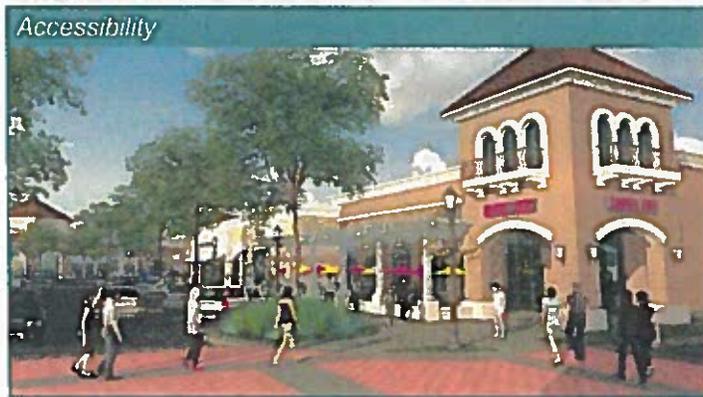
The commercial district buildings in The Point at Harbour View will possess an urban scale and character sensitive to the scale and livability of the adjacent multi-family residential development. The buildings should be designed at a moderate scale appropriate to pedestrians, and should encourage walkability as part of the Mixed Use Core district. Street front functional and visual continuity will be maintained when possible, and all projects should be sympathetic in form, scale, and height to adjacent structures. Structures higher than one story will be designed to minimize their visual bulk and relate to the human scale of pedestrians on the street where possible. Multi-tenant complexes will be designed to emphasize an overall sense of project and place, rather than the prominence of individual tenants when feasible. Buildings along Road A in Area 1 shall be oriented towards Road A providing perpendicular or angled entrances. Double fronted doors will also be allowable in Area 1 along Road A. Other buildings in the development should be oriented to the parking along access roads if feasible. If placement behind structures is not possible or practical, the preferred parking placement is on street parallel or angled to the adjacent structures in order to maintain the presence of active uses along the roads.

Buildings that Relate to a Pedestrian Scale



Building Shape, Form, and Placement

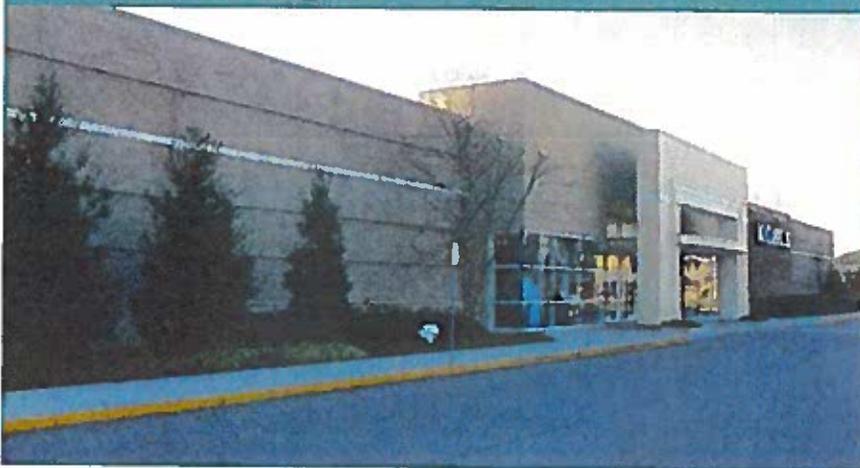
- A. Buildings along Road A in Area 1 shall be oriented towards Road A providing perpendicular or angled entrances. Double fronted doors will also be allowable in Area 1 along Road A. Other buildings in the development should be oriented to the parking along access roads if feasible. Buildings should be located in areas of high visibility from interior roads and should be oriented along the right-of-way while taking into consideration pedestrian accessibility. Frontages should align along the street or parking lot while still allowing varying setbacks for facades and the accommodation of outdoor cafes and public amenities such as plazas and promenades.



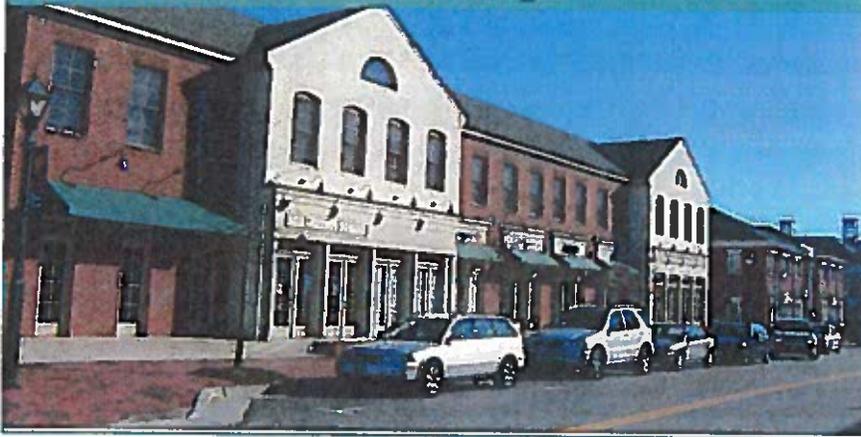
- B. Every effort should be made to space buildings no fewer than 10 feet and no more than 50 feet apart (for gathering areas) when located along an internal roadways. Exceptions can be made with review and approval by the Director of Planning.
- C. Drive-through operations shall be strategically placed to avoid stacking traffic onto adjacent roadways. They should be located on corner parcels when possible.
- D. Buildings will be oriented to avoid blank walls and service areas which are visible whenever possible. When blank walls are unavoidable, add pilasters, trellises, and/or lattices along with planting to make the facades more attractive. The blank wall/space also could be articulated by the provision of blank window openings trimmed with frames, sills, and lintels. See Façade Treatments section. Blank walls in areas 2 & 3 along I-664 are acceptable.
- E. Standalone buildings in Area 1 facing Road A shall have façade treatments on all four sides except for designated rear elevations used for loading and trash pickup. Standalone buildings in areas 2 & 3 shall have façade treatments on 2 sides.
- F. Break overall building masses and large wall surfaces into segments with either varying setbacks, the application of projecting elements onto the façade wall, or through the addition of other architectural elements such as colonnades, loggias, etc. See Façade Treatments section.

2.2 MASSING

Blank Walls are discouraged



Columns, Arches, Arcades shall be encouraged



Developer shall try to use his best efforts to:

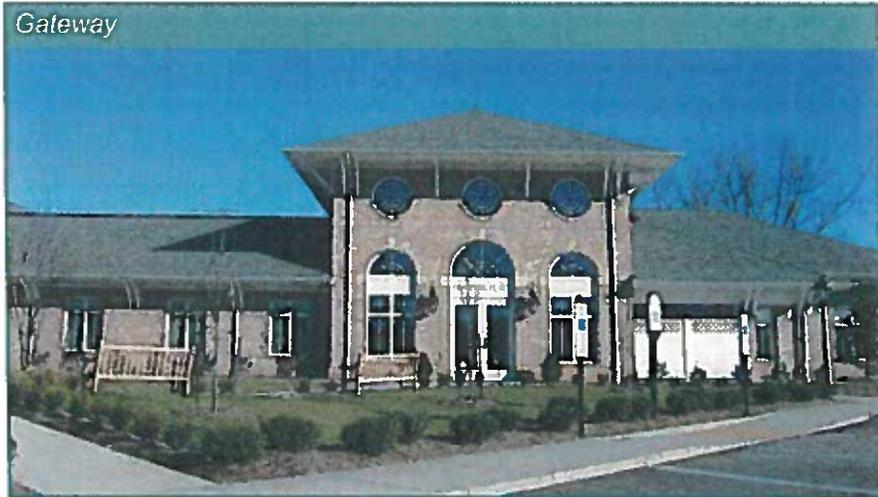
- G. Buildings of smaller mass should be located within the field of the block between larger buildings or anchor tenants. Anchor tenants should be located at corners or along arterials.
- H. Double-fronted buildings are encouraged along the right-of-way.
- I. Building massing to establish gateways in Area 1 along Road A shall be provided. The placement of buildings shall create view corridors into the development.
- J. The placement of buildings at odd or irregular angles to public rights-of-way is discouraged. Where building orientation is placed at an odd or irregular angle to the right-of-way, appropriate pedestrian scale landscape and hardscape features shall be incorporated to create and maintain a walkable and pedestrian-friendly environment.

- K. Corner buildings should take advantage of their prominent locations with angled, rounded, or recessed corner entrances or other small setbacks. Buildings in Area 1 along Road A shall be oriented to public rights-of-way or access roads that share the same context as the public rights-of-way.

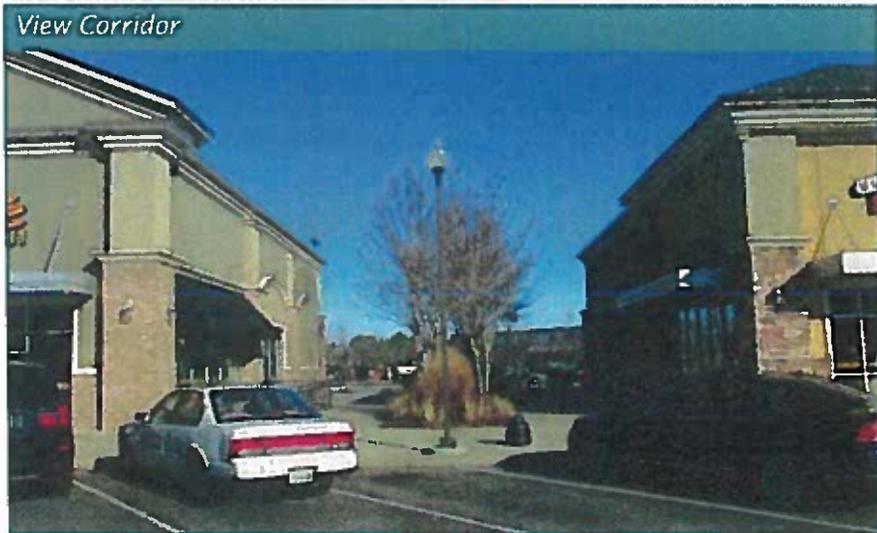
Building Scale

- A. Varied building and parapet heights shall be provided, although extreme differences in building height adjacencies will try to be avoided. There is a need to provide a visual transition between varying heights that contribute to a pleasant overall rhythm of the frontage.

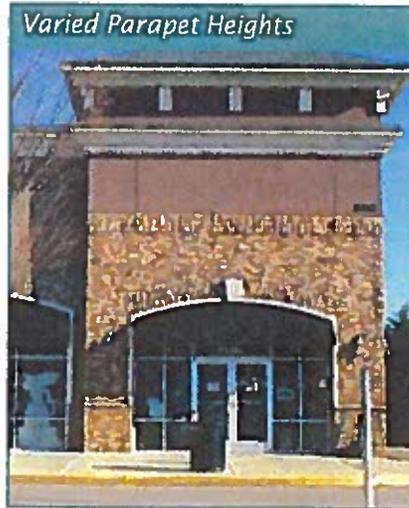
Gateway



View Corridor

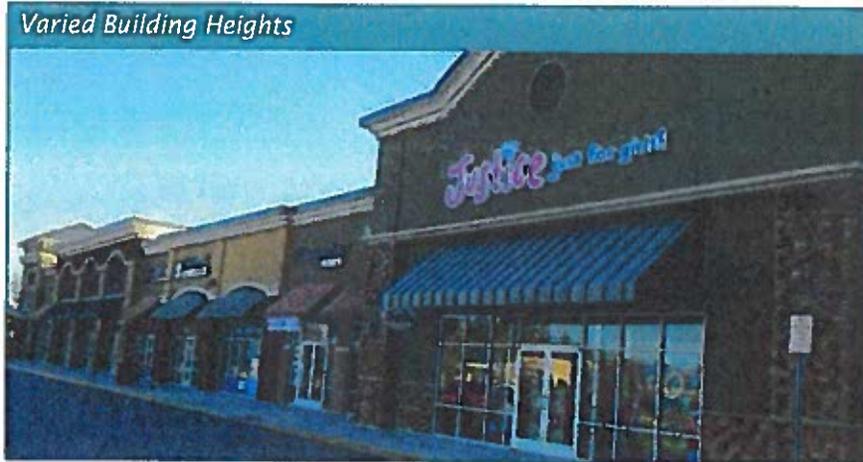


- B. Multistories should be designed to relate to the human scale of pedestrians on the street by using differing façade treatments at the pedestrian level. The greatest level of detail is required at the pedestrian level.



Roof Configuration

- A. All roof equipment must be screened to minimize its visual impact from pedestrian view.
- B. Locate equipment in recessed roof wells or hide equipment behind parapet walls so that it cannot be seen from the main streets.
- C. Roof screens shall be constructed from materials as similar to the building walls as possible, and shall be designed to appear as an architecturally integrated part of the building rather than an added-on element.



RESIDENTIAL USES

The residential portions of The Point at Harbour View will be designed to feel like a neighborhood that is safe and secure, yet allows access to all the amenities and features close to home. New building elements that relate to the size of a person will be used. A visual transition between the commercial district and the residential district will be taken into account when designing for building shape, form, and massing.

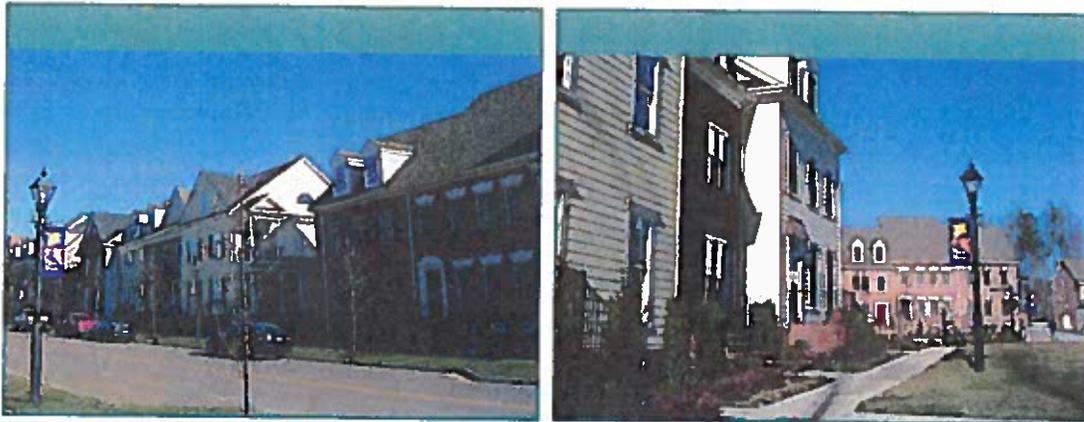
The Developer will work closely with the City of Suffolk Planning to provide an attractive pedestrian scale development to include the below:

- A. Include elements such as bays, trellises, horizontal banding, and ornamentation to provide a sense of human scale. See Façade Treatment section.
- B. Building elements shall visually relate to each other and to the whole building in simple and organized ways. Avoid incongruent arrangements, such as when upper floors bear little compositional relationship to the ground floor, or when multiple roof forms do not relate to how the building is organized.
- C. Multi-family buildings shall be designed at a moderate scale appropriate to pedestrians.

Visual Transition from Commercial to Residential



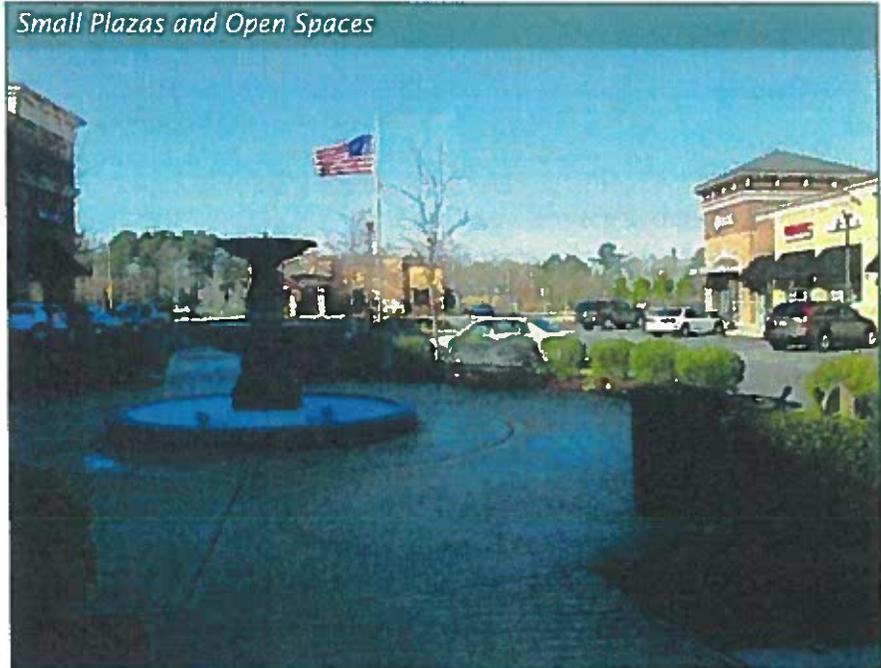
- D. The minimum distance between townhome style buildings shall be 11' and multifamily buildings 20'.
- E. Principal entrances shall be highlighted and the size of openings should generally reflect the size and importance of the associated interior space.
- F. Porches and balconies should be used to serve as a transition between public and private spaces.



February 12, 2018

- G. Selected small plazas and open spaces may be provided for gathering areas, and pedestrian connectivity to both the residential area and the commercial development should be included.
- H. Buildings should be generally oriented along the drive aisles while taking into consideration pedestrian, accessibility, and amenities such as plazas and promenades.
- I. The placement of buildings shall create view corridors into the development.
- J. The placement of buildings at odd or irregular angles to rights-of-way is discouraged. Where building orientation is placed at an odd or irregular angle to the right-of-way, appropriate pedestrian-scale landscape and hardscape features shall be incorporated to create and maintain a walkable and pedestrian-friendly environment.

Small Plazas and Open Spaces



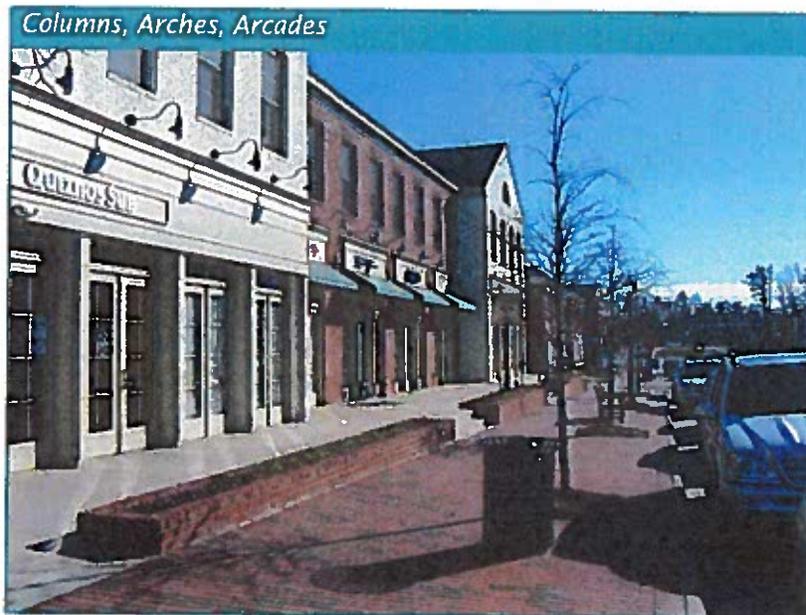
Façade Treatment

NON-RESIDENTIAL USES

Buildings are encouraged to be mainly of masonry construction (cement board, brick, or stone), with window and door openings designed as individual units. The main facades should incorporate architectural elements that create a better human scale. These elements can include but are not limited to columns, arches, arcades, and articulation of individual storefronts within the larger facade. Elements which enhance the pedestrian walking zones such as arcades, overhangs, and awnings are strongly encouraged. Buildings throughout the commercial district should face the street, with strong pedestrian orientation, providing opportunities for window shopping, outdoor dining, and clear, inviting front doors.

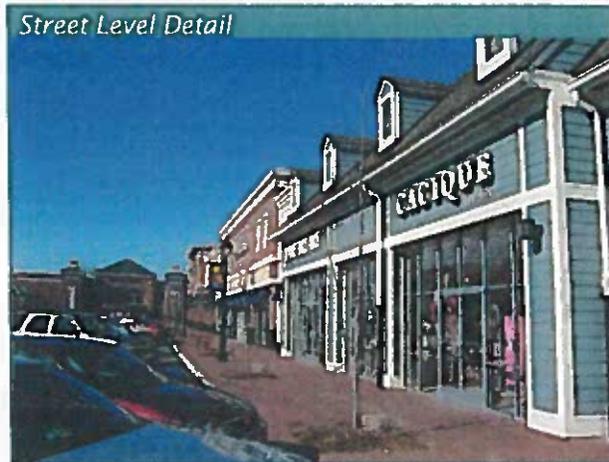
Styles

- A. Maintain continuity of design, materials, color, form, and architectural detail for all elevations of a building that are visible from public areas or adjacent residences.
- B. Where continuity of design is difficult to achieve, substantial planting and decorative screening walls will be provided to screen the area.
- C. The greatest amount of detail should be presented at the street-level. Features that may be incorporated include overhangs, raised cornices, parapets, arcades, arches, canopies, recesses or projections, display windows, etc.



3.1 FAÇADE TREATMENT

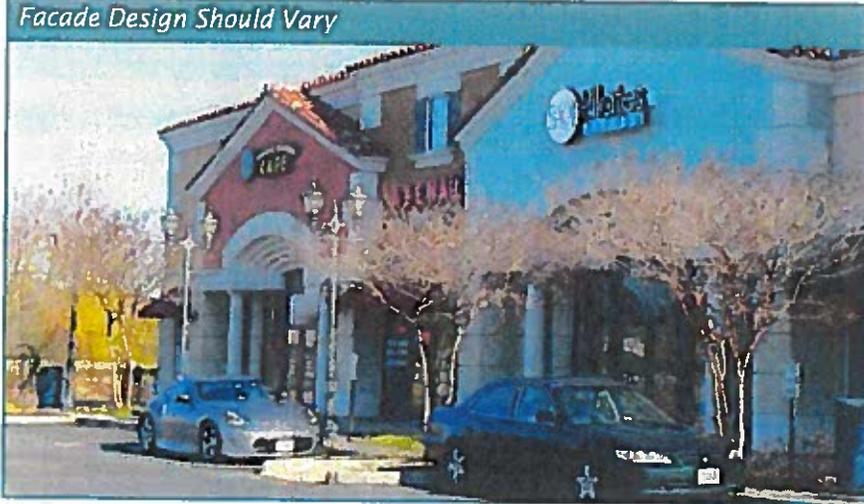
- D. Flat roofs located behind parapet walls are encouraged. These roofs provide a location for mechanical equipment where it can be hidden from view. Designers are encouraged to articulate the parapet wall as of a means of adding interest to the building facade. Other roof configurations, including pitched roofs, shed roofs over arcades, and pitched roofs on entry-point towers are also encouraged.
- E. Use applied and integrated design elements, such as exposed rafter tails on sloped roofs, cornice moldings, or applied medallions.



Frontage and Fenestration

- A. Consistent fenestration among buildings should be maintained. Rhythms carried throughout the block, such as those established by storefront, window, and door spacing, should be kept similar to other rhythms of adjacent buildings.
- B. Building design shall provide a base, middle, and top. As well as providing visual order to the building, this encourages a pedestrian scale and a sense of comfort to the pedestrian.
- C. The height of the base should be proportional to the overall height of the building.
- D. Façade design should vary, rather than presenting a single face along the frontage.
- E. Details per each section shall include:
 - 1. Area 1 along Road A shall consist of customer access along Road A, either parallel to the road or angled. This shall also include a 50% minimum of windows, doors, and other fenestration elements along Road A. This shall be measured from base of building to 16 ft. in height.
 - 2. All other sections of Area 1 and Area 3 shall commit to a minimum of 30% fenestration and frontage design elements along the most prominent and main entrance. This shall be measured from the base of the building to 16' in height.

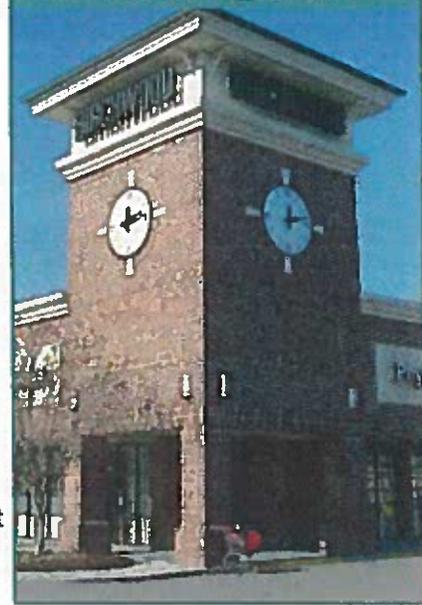
3.2 FAÇADE TREATMENT

Storefront Rhythm*Facade Design Should Vary*

- F. Windows, doorways, bays, and pediments should not vary significantly in size from windows, doorways, bays, and pediments in adjacent buildings.
- G. The size of facades facing the public right-of-way or parking lot should be similar in area and height-to-width ratio. The largest façade should not vary significantly in size from the smallest façade unless there are multiple stories.
- H. Buildings on corner lots shall address the corners with main entrances, chamfered, or curved building corners. Articulating the building corners with windows, towers, turrets, etc. is encouraged.

3.3 FAÇADE TREATMENT

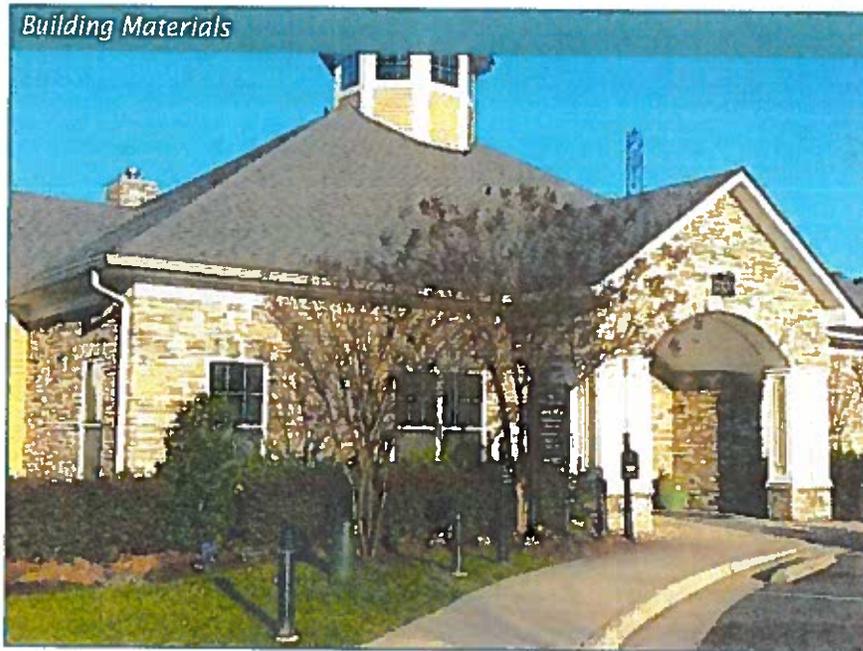
- I. Buildings in Area 1, along Road A, shall have their main entrances along the street or at building corners on a 45 degree angle, but double-frontage buildings are allowed and encouraged.
- J. The front doors of buildings shall reflect the scale, placement, and proportions of adjacent buildings; recessed entrances are encouraged.
- K. Doors and entryways of buildings shall be compatible with the overall style and character of the façade.
- L. Entry doors should be transparent whenever possible.
- M. Windows shall complement the architectural style of the building.
- N. Ground floor windows can be used for displays. However, business owners should allow full and unobstructed views into their businesses. Completely obstructing windows from the interior of a building with shelving, display cases, signage, or other objects will be discouraged. Instead, low or pass-through displays will be designed.

Articulated Building Corner

Materials

- A. The buildings shall be constructed of masonry or masonry looking building materials such as stucco, brick, architectural concrete (e.g., architectural grade pre-cast concrete, cast stone, or pre-cast concrete finished with an elastomeric coating system), or natural stone. These materials should be used on all sides of the building. Many other high-quality materials are also allowed, such as ceramic tiles and high quality coated architectural metal panel systems (above the first story only).
- B. Secondary or accent exterior building materials may be stainless steel, copper, bronze, brass, or painted steel. Mortar and caulking colors should be compatible with the predominant material.

3.4 FAÇADE TREATMENT



- C. Certain materials are discouraged as wall materials, including woods that will rot, fluted block, EIFS, or Dryvit (except on upper story or service areas).
- D. Doors and windows should be glass and aluminum (painted) storefront, high-quality steel and glass systems, high-quality woods such as plantation grown teak or mahogany, and brass, bronze, or stainless steel.
- E. Window glazing should be clear or slightly tinted.
- F. Flashing, gutters, and downspouts are encouraged to be designed as one internal, non-visible system of the building.
- G. Colors selected will be that of the materials permitted above, or a range of earth tones achieved from the use of the appropriate building components.
- H. Color shall be used to unite the elements of a façade and to highlight architectural features. However, the colors on individual buildings should complement and be compatible with the predominant hues of nearby buildings. Darker or lighter hues may be used as accent trim.

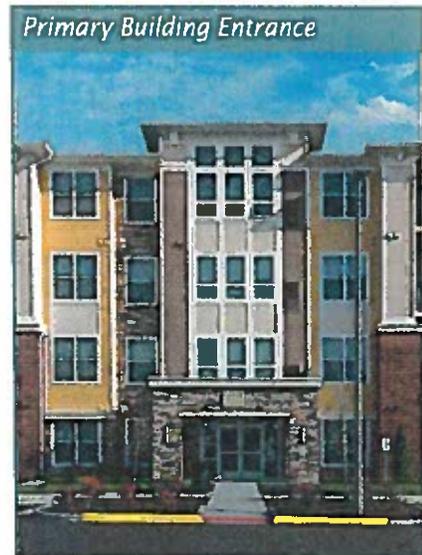
3.5 FAÇADE TREATMENT

RESIDENTIAL USES

Residential uses are encouraged throughout The Point at Harbour View. Primary building entrances should be at or near the sidewalk. Residential entrances that face streets or pedestrian paths, whether shared or for individual dwellings, shall be accompanied by a covered porch, stoop, veranda, or other features that highlight entry points, offer rain protection, and encourage interaction between neighbors.

Frontage and Fenestration

- A. Primary building entrances will be visibly emphasized, architecturally embellished, accessible from a street, and provide a sidewalk.
- B. Entrances that cannot face a public street and sidewalk shall face an internal pedestrian path that connects directly to a street or parking area and sidewalk. Internal pedestrian paths may be accompanied by courtyards or plazas.
- C. Visually prominent exterior corridors that provide access to multiple units on upper floors are discouraged.
- D. Front doors (individual or shared) should have a substantial appearance. Avoid flush face doors. Front doors leading to the outside shall generally incorporate panels, windows, or be accompanied by adjacent windows so occupants can see out.
- E. Railings, balustrades, posts, and columns should be similar in form and color throughout the development. Posts and columns should have a substantial architectural appearance.
- F. Gabled, hipped, or shed roof types should be used and may be accompanied by dormers.
- G. Eaves should be incorporated into the design to create shadow. Deep eaves are encouraged when gabled, hipped, and shed roofs are used.



3.6 FAÇADE TREATMENT

- I. Long building fronts will use projections or recesses, compose windows, space chimneys, and/or use other devices to set a rhythm at smaller intervals.
- J. Bay windows, cornices, and other architectural projections are encouraged.
- K. All buildings will try to clearly express a base, middle, and top.
- L. The design of the base should convey its loadbearing function through the use of materials such as brick or stone, darker colors, or deep joints in masonry or stucco.

Base, Middle, Top Clearly Expressed



- M. The top creates a prominent visual termination for the building, and can add interest through carefully considered roof forms, cornices, eaves, and parapets. Roof pitch, and its materials, size, and orientation are all distinct features that contribute to the character of a roof.
- N. Uninterrupted exterior surfaces exceeding 50% of the total span are prohibited. Blank walls devoid of fenestrations or architectural design elements are discouraged.
- O. Clear glass windows should face streets, plazas, courtyards and/or pedestrian passages. Recess windows from façade or trim to add shadow and visual interest whenever possible.
- P. Mechanical equipment shall be screened from nearby pedestrian view.

3.7 FAÇADE TREATMENT

Materials

- A. High-quality, durable materials that age well shall be used. Materials and applications that will discolor should be avoided.
- B. Exterior finishes shall be wood, brick, stucco, architectural pre-cast stone, masonry, natural stone, cement board (e.g., Hardy-plank), or heavy vinyl. Metal panels (including corrugated metal), reflective glass, and glass curtain walls should not be used.

Exterior Finishes



- C. Select base colors that blend with the predominant colors and features of surrounding buildings and landscape. Don't detract from buildings in the vicinity, especially if they are designated as architecturally or historically significant.
- D. Bolder colors shall be reserved as accents for building details, ornamentation, or special features to reduce the perceived bulk of structures.
- E. Consider articulating or emphasizing building corners with quoins, medallions, brick, or stonework.
- F. Double-hung, awning, or casement type windows are preferred. Window frames should be wood, vinyl, fiberglass, or colored clad metal. Bare metal should not be used except when used intentionally as a modern, industrial, agricultural, or vernacular expression. Glass should be clear and non-reflective, and mirrored glass should not be used.

3.8 FAÇADE TREATMENT

Streets

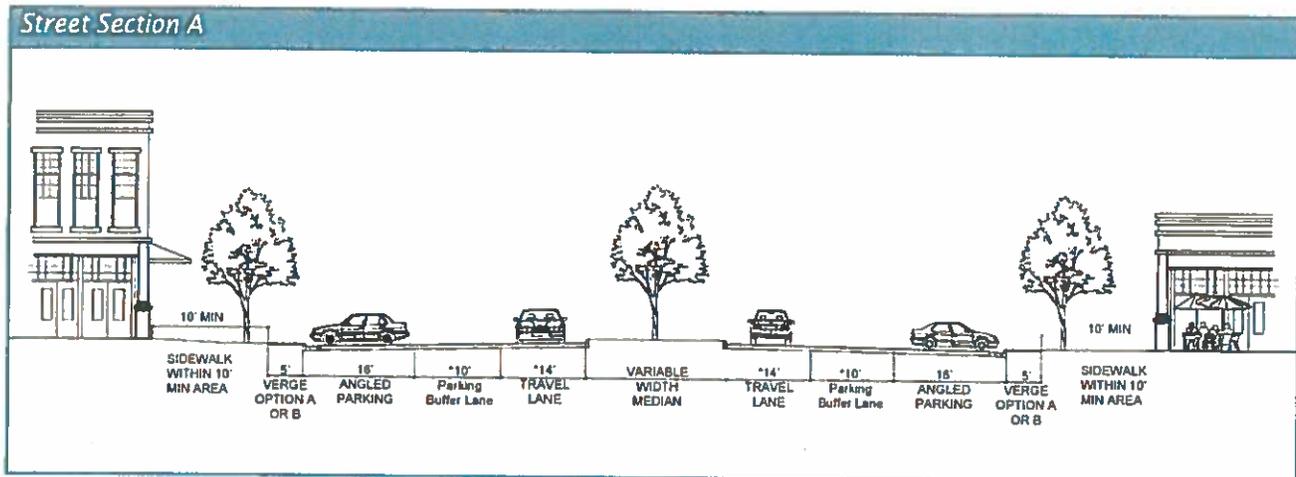
The Point at Harbour View will have three major street types with their own signature look and feel. Additional travel ways within the residential and non-residential areas of The Point at Harbour View will be accommodated with the use of typical parking lot drive aisles.

It shall be determined if roadways will be private or public through final coordination and approval with traffic engineering during site plan approval.

Hardscape, i.e. irrigation, site furniture, etc, and landscape, shall be maintained by a property owners association or private entity.

These three street types are:

- Street Type A: 4 lanes with median and angled parking
- Street Type B: 4 lanes with median and parallel parking
- Street Type C: 2 lanes with parallel parking



A. Street Type A

Street Type A is proposed at the entrance to the development from College Drive. The following standards will be in addition to the City of Suffolk's standards for roadway construction and plant material:

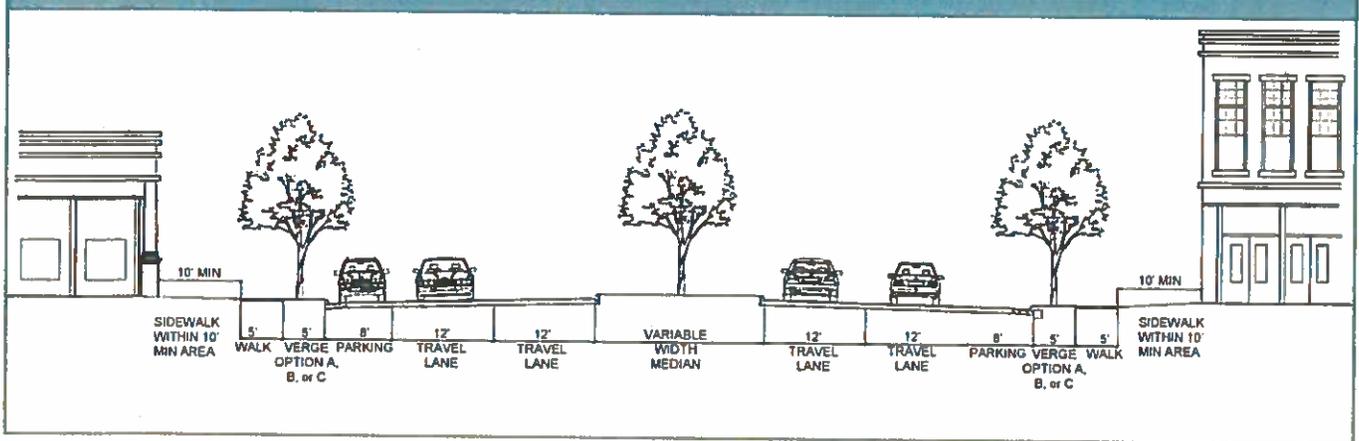
1. The street will consist of four main travel lanes. Turn lanes will be incorporated as required. Angled on-street parking will be provided on both sides. These lanes will augment the site's parking needs and act as traffic calming. * Final lane dimensions to be finalized with coordination with Department of Public Works during plan approval process.
2. A median will vary in width as necessitated by traffic control. Vegetation should be applied for back-of-curb to back-of-curb distances greater than 3 feet, 6 inches wide. Widths less than 3

feet, 6 inches will be infilled with concrete flush with the top-of-curb per standard roadway construction techniques. Shrub and groundcover plant material will not exceed 2 feet, 6 inches in height, either as a product of pre-existing maintenance guidelines or in their natural, mature growth. Trees with canopies will not be greater than 50 feet in mature spread and will be limbed to maintain views. Vegetation will be placed to encourage the use of crosswalks but not to restrict emergency pedestrian egress from travel lanes. Signage and utility pedestals are permitted in support of The Point at Harbour View.

3. A verge not less than 5 feet wide with plantings and walk areas will be designated behind the back-of-curb. Standard roadway appurtenances such as signs and utility pedestals may occupy this zone. The slope between sidewalk and back-of-curb will not exceed 5 percent and turf will be flush with both sidewalk and top-of-curb with no mounding or berming in between. Lead walks through the planted verge may be provided. (See verge diagram 4.5 Streets)
4. The zone between the sidewalk and on-site improvements will consist of walkway, utilities, tree, shrub, and groundcover planting as necessary for screening or aesthetic structure and signage. Items to be screened will include not less than three tiers of shrub and groundcover material with the species closest to the screened item being the tallest in its habit and evergreen in its persistence.
5. Crosswalks will be utilized as required. The finish and pattern used on the sidewalk should continue onto the curb ramp with the exception of the detectable warning devices prescribed by accessibility standards. For paired ramps at an intersection, a two curb ramp (non-shared) configuration will be employed as a measure of safety. With any curb ramp, flared sides will only be employed when those flared sides abut a paved walking surface at the top edge. A curb-return (non-flared side) configuration will be used otherwise. Crosswalk finishes within the travel way will consist of stamped concrete or concrete or brick paver configuration in a continuous herring bone field and of a color distinguishable from the travel lane pavement. Crosswalks should continue through the nose of the median in lieu of running in front of the nose to provide an additional measure of protection for pedestrians.
6. Parking design will provide for car positions that do not allow for car bumper encroachment into a sidewalk.

4.2 STREETS

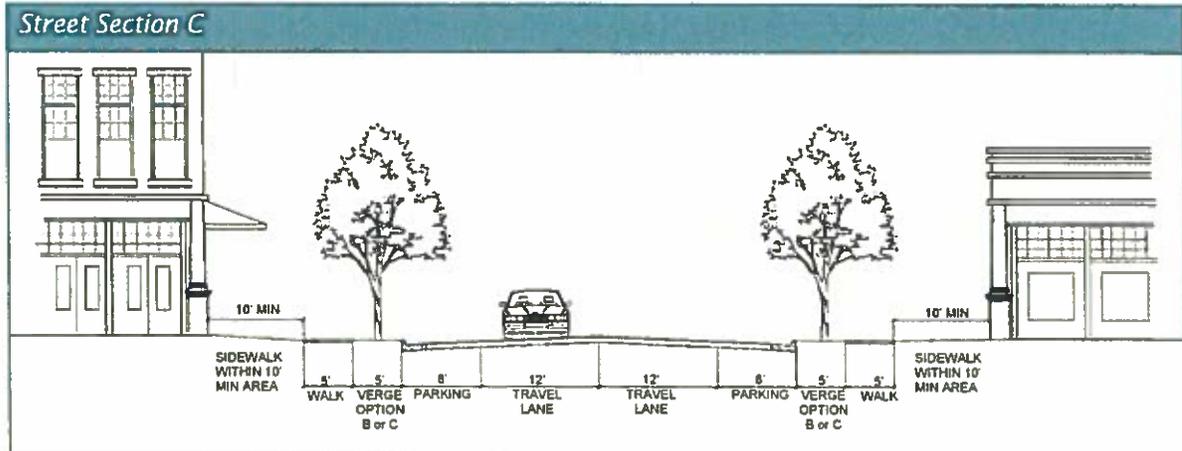
Street Section B



B. Street Type B

This street type will be the primary vehicular entrance off Armstead Rd. Two travel lanes and parallel parking will be provided. The following standards will apply to Street Type B:

1. The street will consist of four main travel lanes. Turn lanes will be incorporated as required. Parallel on-street parking will be provided on both sides. These lanes will augment the site's parking needs and act as traffic calming.
2. Median construction along the central access road will commensurate with standards established in item A.1.
3. A verge not less than 5 feet wide with plantings and walk areas will be incorporated between back-of-curb and sidewalk. This zone will be populated with tree species having canopies not exceeding 50 feet in mature spread, shrubs, and groundcover plantings as appropriate for the width at a given point. Signage and utility pedestals are permitted in support of The Point at Harbour View. (See verge diagram 4.5 Streets)
4. A sidewalk not less than 5 feet wide will be constructed from intersection to intersection on both sides of the travel way. This walk will be constructed with a concrete or brick paver. A brick paver configuration will be in a continuous herring bone field, and bound with a minimum 9 inch concrete band flush with the paver surface and surrounding grade.
5. The zone between the sidewalk and adjacent improvements will consist of lateral walks connecting to adjacent walks or building aprons; utilities in support of The Point at Harbour View improvements; appropriately sized tree, shrub, and groundcover planting as necessary for screening; adorning building foundations; or aesthetic structure. Signage also is permitted. Items to be screened will incorporate species evergreen in their persistence and appropriate for the height of the item being screened.



C. Street Type C

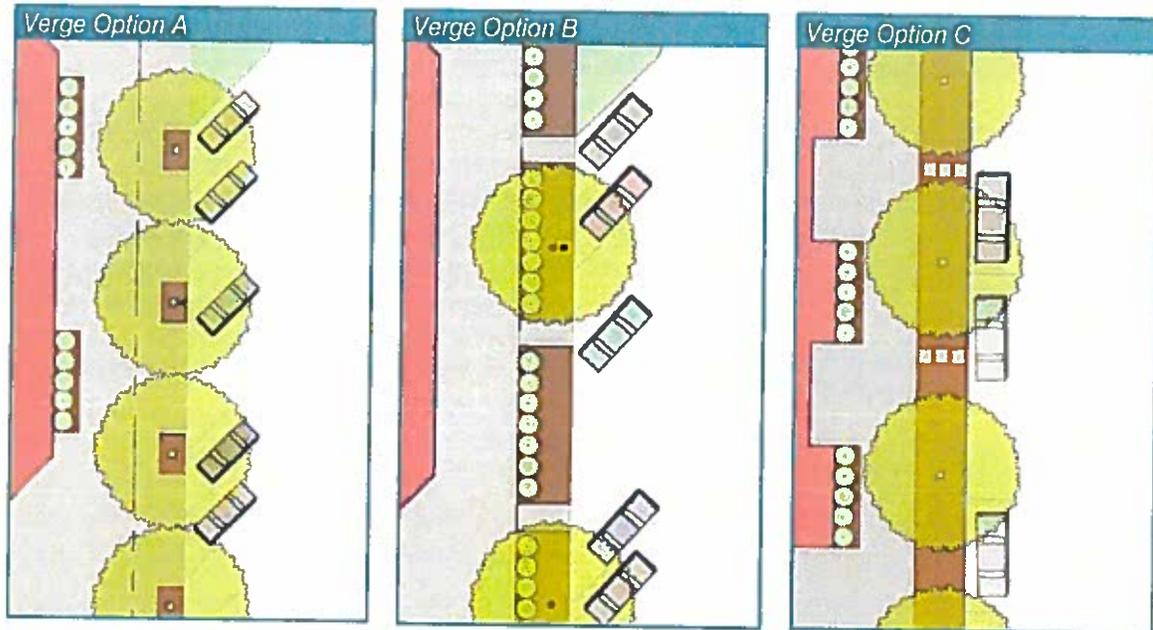
Street Type C will be the primary road type for the remainder of the development and will have the following standards:

1. The street will consist of two main travel lanes. Turn lanes will be incorporated as required. Parallel on-street parking will be provided on both sides. These lanes will augment the site's parking needs and act as traffic calming.
2. A variable width, vegetated verge will be incorporated between back-of-curb and sidewalk. This zone will be populated with tree species having canopies 50 feet in mature spread, shrub, groundcover, or turf plantings as appropriate for the width at a given point. Concrete infill is not permitted. (See verge diagram 4.5 Streets)
3. A concrete sidewalk not less than 5 feet wide will be constructed on both sides of this verge.
4. The zone between the sidewalk and adjacent improvements will consist of lateral walks connecting to adjacent walks; utilities in support of The Point at Harbour View improvements; appropriately sized tree, shrub, and groundcover planting as necessary for screening; adorning building foundations; or aesthetic structure. Signage and utilities also are permitted. Items to be screened will include not less than three tiers of shrub and groundcover material with the species closest to the screened item being the tallest in its habit and evergreen in its persistence.
5. Crosswalk construction will commensurate with standards established in item A.4.

4.4 STREETS

D. The following items apply to all street types:

1. Standard roadway appurtenances such as signs and utility pedestals will be placed in accordance with the Signs section and the Utility section of this guideline, plus the City of Suffolk and VDOT standards.
2. Pavement surfaces will be constructed in accordance with current City of Suffolk Public Works standards.
3. Utilities will be screened with respect to clear zones, if applicable.
4. Site distance triangles will be recognized at all intersections.
5. Planting shall meet or exceed the Landscape Design section of these guidelines.
6. Planting guidelines will be in addition to the City of Suffolk Landscape standards when placed within public right-of-way. Street trees will be placed within public right-of-way in accordance with City prescribed street tree species. Where conflicts between these guidelines and City right-of-way standards exist, City standards will prevail.
7. Final travel lane configuration will be subject to the approved Traffic Impact Study, which will take precedence over this Design Criteria.
8. Crosswalks should continue through the nose of the median in lieu of running in front of the nose to provide an additional measure of protection for pedestrians.
9. Sections depicted are recommended but are subject to modification to meet site development needs as approved by the Public Works Director.

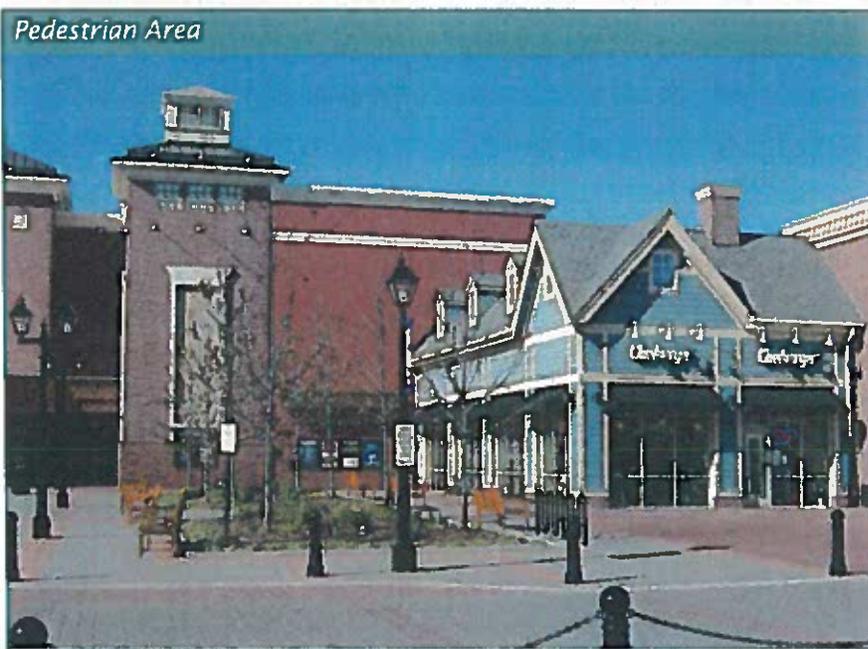


Pedestrian Orientation

The non-residential areas of the development may be a mix of single and multi-story buildings. In multi-story buildings, retail should be placed on the first floor to encourage pedestrian access. In single-story buildings, pedestrian access is implied.

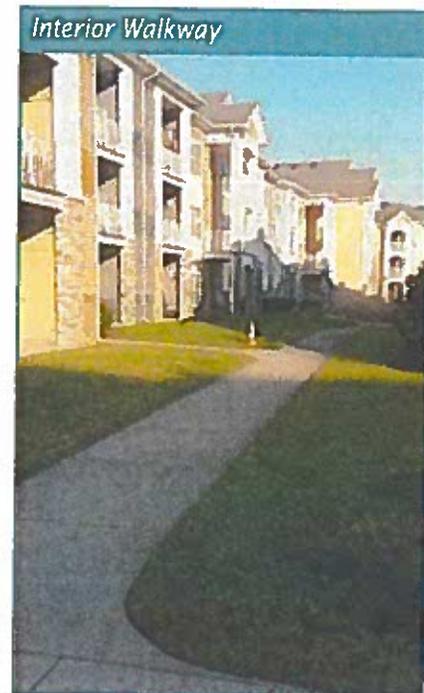
The residential development will include both three- and four-story buildings. Second-story and above levels will be made accessible with the use of staircases in each building. Elevators are encouraged for enhanced accessibility to second-story and above levels.

For any use, ADA guidelines will further ensure ease of pedestrian movement both within the building, the building's curtilage, and within the paved surfaces on site.



A. Buildings

1. Buildings entrances will be located within 75 feet of any parking area.
2. Access to buildings from exterior paved surfaces will comply with ADA guidelines and International Building Code (IBC) regulations.
3. All buildings, regardless of use, will be connected with a system of walks, crosswalks, ramps, and building aprons.



B. Walks

All walks within and outside of right-of-way will be constructed in accordance with current City of Suffolk Public Works standards.

1. Walk material will include concrete or brick paver configuration in a continuous herring bone field, and bound with a minimum 9 inch concrete band flush with the paver surface and surrounding grade.
2. As an alternative, standard concrete walks may be constructed. Efforts should be made to include paver accenting within standard concrete walk construction.
3. Walks shall be a minimum of 5 feet wide.
4. Walks will not include the top-of-curb in their width calculation when they abut curbing.
5. Every effort will be made to ensure no walk exceeds a 1:20 longitudinal slope. Where walks exceed this slope they will be fitted with a handrail.
6. No walk will exceed a 1:12 longitudinal slope.
7. Walks will be laterally sloped sufficient to ensure proper drainage.
8. Walks will be constructed and maintained free of obstructions and trip hazards.

5.2 PEDESTRIAN ORIENTATION

C. Other Paved Surfaces

Other paved surfaces include but are not limited to courts, aprons, plazas, or any paved surface intended for pedestrian use and conveyance that are not considered walks. All paved surfaces outside of the right-of-way will be encouraged to use the City of Suffolk Public Works standards.

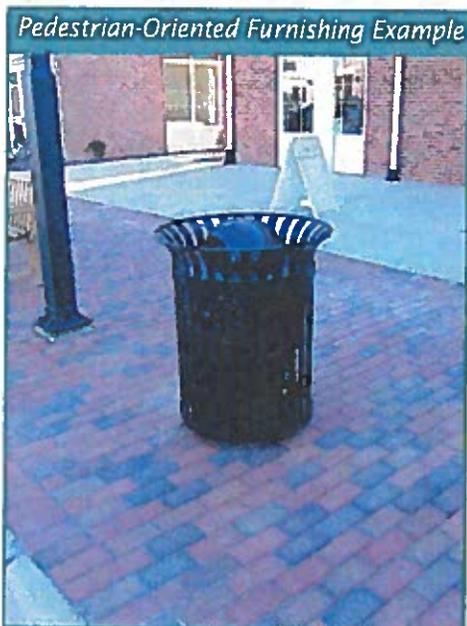
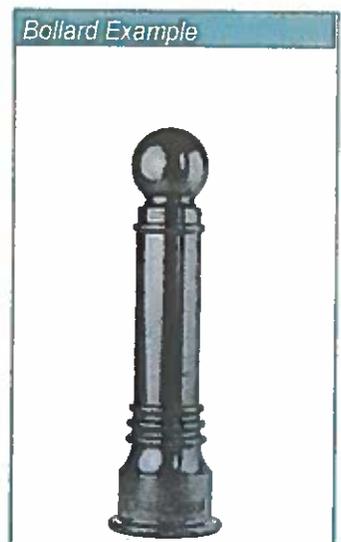
1. Surfaces used will not exceed a 1:20 slope and will ideally conform to a range of 1:50 to 1:33 slope.
2. Surfaces intended for pedestrian use will be sloped sufficient to ensure proper drainage.
3. Surfaces will be constructed and maintained free of obstructions and trip hazards.
4. Stamped pavement in the public right-of-way should be color-dyed concrete.



5.3 PEDESTRIAN ORIENTATION

D. Outdoor Furnishings and Amenities

1. Pedestrian-oriented furnishing and amenities such as benches, trash receptacle, and pedestrian lighting will be placed judiciously throughout The Point at Harbour View in accordance with standard design practice. Refer to the Utilities section for additional guidelines on lighting.
2. Furnishings and amenities will be selected with pedestrian comfort and convenience in mind. Style can vary between the residential and non-residential areas only. See the Structures and Furnishings section for guidelines on furnishings.



Ground Floor Uses

For commercial buildings containing multiple stories, the ground floor is typically reserved for commercial, retail, and office uses. The upper stories are typically residential but also can be used for commercial and office uses. Corner and end-unit buildings should be larger tenant anchors. Regional uses and smaller, neighborhood-oriented stores can be accommodated in the The Point at Harbour View commercial district. In the residential district, ground floor residential units will try to be elevated above finished grade to provide more privacy from the street and sidewalk.

NON-RESIDENTIAL EXAMPLES

- A. Mixed-use
- B. Office
- C. Neighborhood and regional scale retail
- D. Community facilities
- E. Civic buildings
- F. Outdoor common areas
- G. Hotel

RESIDENTIAL EXAMPLES

- A. Multi-family
- B. Single-family attached
- C. High-density single-family detached
- D. Outdoor common areas
- E. Clubhouse and resident amenity facilities

The designations above represent broad use categories. Please see the Land Use section of these guidelines for a complete list of specific uses that may be appropriate for the ground floor.

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Access

The Point at Harbour View will be accessed predominantly via personal automobile from Street Sections A and B. Once on site, pedestrian circulation will become the primary means of accessing various amenities and facilities.

A. Auto Access

Automobiles can primarily access The Point at Harbour View via entranceways which are sited off College Drive and Armistead Road. There will be three primary road types within the site.

1. Section A will be the primary access to the center of the project. An inviting 4 lane divided roadway with angled parking will allow for pedestrian and vehicular traffic to intermingle easily. As you approach the center of the site a roundabout will create a central point for decision making.
2. Section B will designate your approach to the commercial and residential portions of the mixed use development. Parallel parking will line the 4 lane divided roadway. These roadways will be lined with buildings that can be easily accessed by pedestrians.
3. Section C roadways shall promote pedestrian access as well. These roadways are the secondary access points and shall separate uses on site. Parallel parking and 2 lanes of traffic make these roadways pedestrian friendly. The connectivity for both vehicles and pedestrians will be continuous throughout the site with seamless transitions from sections 1, 2, and 3.
4. Parking lots will have designated entrances along the internal roads. Appropriately sited ADA-compliant parking spaces will provide no less than the minimum required number of accessible spaces in the parking lots per ADA guidelines.

B. Public Transit

A bus route will introduce an alternate option for site access.

1. The bus route will be located along Street Section A & B.
2. The bus stop will be designated with adequate signage, a noted bus schedule, an open fronted shelter, and appropriate seating.
3. A City bus should access the designated stop multiple times throughout the day, and should follow a route that leads to places of interest and connects with additional City bus routes.
4. The bus stop and City bus will be compliant with any and all ADA accessibility requirements.

C. Pedestrian Access

The Point at Harbour View should be as walkable as possible to encourage use by pedestrians.

1. Parking lots shall have designated and apparent pedestrian walkways to guide users through the lots to site amenities.
2. Curb cuts and ramps should be appropriately placed to increase ease of access for users with disabilities.
3. Sidewalks and crosswalks will provide an unobstructed path guiding pedestrians to site components such as residential and outparcel structures, public transit areas, seating areas, bike racks, open spaces and alike.

Example of connection in Area 3 for prominent buildings



Pedestrian Path through Parking



Curb Cut Ramp

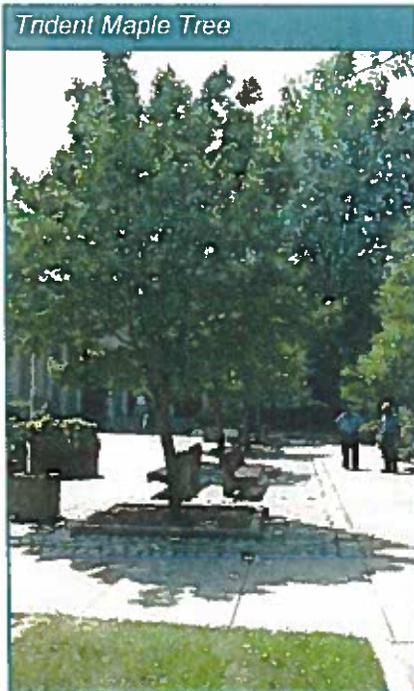


Landscape Design

TREES

Trees are an integral part of any development since they provide scale, spatial definition, shade, groundwater stabilization, evapotranspiration, and other social and ecological benefits to a site. Trees bring a remarkable sense of value to any real estate, and depending on the species selected and their location, they can extend the architecture of the building into the site and make outdoor spaces generally more interesting and enjoyable. Trees should be regarded as essential building materials for any parcel or residential land development. The Developer will use his best efforts to use the following standards for trees in The Point at Harbour View:

- A. Trees shall be planted on an established spacing and coordinated with the pavement design and lighting.
- B. Trees will be located to promote shade and to create a uniform street edge or pleasing composition for the overall site.
- C. Street trees shall be spaced evenly along the right-of-way and spaced at regular intervals at no more than 50 feet apart.



8.1 LANDSCAPE DESIGN

- D. For areas where trees are restricted to tight spaces, the use of structural and horticultural soils will be used to promote better tree health. A root barrier will be used where trees come closer than 4 feet to a sidewalk or curb to reduce root upheaval and cracking of the pavement structures.
- E. Trees planted along Street A and B and in Area 1 shall be a minimum of three (3) inch caliper at time of installation. All other plant sizes at a minimum shall follow the City's Landscape Standards. Refer to Article 6, Section 31-603 of the Unified Development Ordinance.
- F. Small ornamental and flowering trees should be spaced no more than 20 feet apart.
- G. When possible, small ornamental trees and columnar-shaped trees should not be used in parking lot islands since they do not efficiently shade the pavements as compared to the larger street trees.
- H. Plantings and trees in particular, should be coordinated with lighting and underground utilities.
- I. Mulch rings or earthen saucers around trees should be eliminated when trees are installed within plant beds.
- J. Tree root crown should be located no more than 1 inch above the adjacent mulch grade.
- K. Trees will be fully installed within a mulch bed and not mounded up significantly higher than adjacent grade.
- L. The City's Unified Development Ordinance, Article 6, Section 31-603 Landscaping Standards are incorporated into these guidelines by reference.
- M. Existing vegetation on site shall be retained for landscaping and screening when practical.

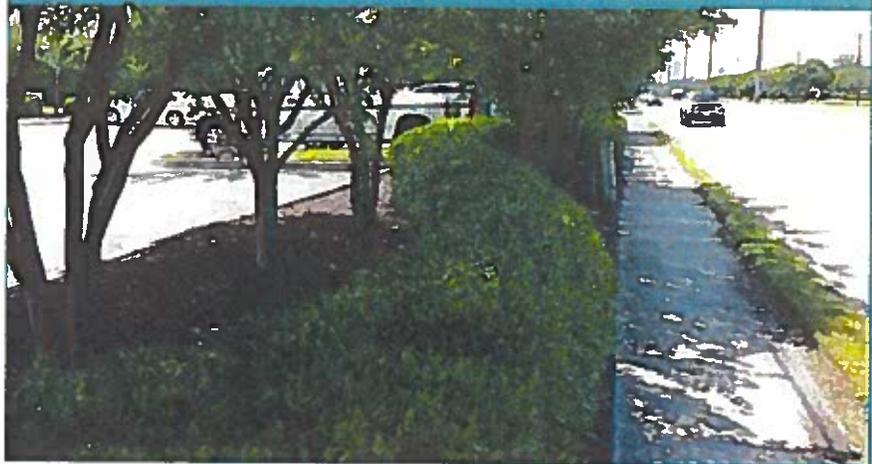
SHRUBS, GROWDCOVERS, PERENNIALS, AND ANNUALS

The plant palette for general development shall be simple and consistent. It shall be supplemented with other plant options for interest, but some basic plant species and compositions should be carried throughout the site. Below are some basic requirements for shrub plantings:

- A. Each plant bed shall have a low evergreen shrub as a foundation to the planting composition.
- B. Plants should be arranged in masses and should be maintained to preserve the original design intent—i.e., shrubs shall not be sheared individually when in hedge form or in a mass of same shrub species.
- C. Shrub bed finished elevations will be kept equal to the turf surrounding it. In cases where they are adjacent to pavements they will be recessed by at least 1 inch to help avoid or slow down the buildup of mulch from seasonal topdressing.

Knockout Rose Shrub

- D. Planting shall be staged in order to provide adequate scale to the plant bed and allow visibility of plantings behind front rows of the bed.
- E. Pruning and maintenance shall be considered during design in an effort to promote selective pruning versus indiscriminate topping of small trees or shrubs.
- F. Ornamental grasses are encouraged, provided they are not overly used. These plants can complement the evergreen plant material in the winter months and provide a sense of movement from windblown tassels.
- G. Mature sizes of plants shall be considered during design to ensure that overcrowding of plant masses don't become a problem with pests, disease, and unsightly patches where plants have to be removed prematurely.
- H. Perennials are encouraged at special areas in shrub beds, but they should not remain the key plant in any composition due to their seasonal nature.
- I. Annuals shall be considered as part of the planting composition in the most active areas and visible places in The Point at Harbour View. Their color and seasonal replacement will ensure interest for people who live, work, or visit the development site.

Staged (Tiered) Planting*Continuous Hedge*

- J. Plants will not be socket-planted but will be installed in a continuous bed, excavated, and backfilled with prepared soil mixtures.

8.4 LANDSCAPE DESIGN

TURF

Turf may be used sparingly on this site, and smaller quantities should be implemented using sod. The following design guidelines should be adhered to when using turf at The Point at Harbour View:

- A. Turf will be Common Bermuda or a City-approved cultivar thereof.
- B. Sod should be used for small areas for immediate impact.
- C. If seeding a larger area of turf, hydro-seeding may be considered as an effective way to encourage germination and rapid establishment of a turf crop.
- D. All turf shall be graded to drain onto itself, and all pavements, where practicable, should drain onto turf in order to reduce potential freeze over of sheet flow during inclement weather and to avoid sedimentation onto pavements during heavy rains.

Staged (Tiered) Planting - Residential Area



IRRIGATION

Irrigation can be beneficial for planting compositions, particularly at the beginning stages of plant growth. Irrigation guidelines for The Point at Harbour View are:

- A. Irrigation shall be considered in the most public areas of a site, such as an entrance area, active open space, or foundation planting at building frontages.

- B. Irrigation should be designed with manageable zones visible from the controller and turf areas should be separated from shrub beds. Irrigation types also should be separated by zone, such as drip irrigation separated from spray heads.
- C. Irrigation will be sourced by a meter coordinated with individual taps.
- D. Spray should be controlled and aimed carefully away from pavements.
- E. Drip emitters and hoses should be buried entirely below the mulch in plant beds, and observation wells should be placed strategically to confirm proper function of the drip lines without having to unbury the line.
- F. Private irrigation systems used in the public-right-of-way will be owned, operated, and maintained by the property owner's association and will be subject to approval by Public Works.

PLANT PALETTE

Plant materials selected for developments within The Point at Harbour View may vary from one development site to another, but an overall basic set of plants should resonate throughout the entire project area in order to help unify one area to another. The following plant palette is presented as a general starting point for development planting plans and alternatives may be approved as provided for in Appendix C of the Unified Development Ordinance:

Large Canopy Trees	
<u>BOTANICAL NAME</u>	<u>COMMON NAME</u>
<i>Acer rubrum</i>	Red Maple
<i>Acer saccharum</i>	Sugar Maple
<i>Fraxinus pennsylvanica</i>	Green Ash
<i>Ilex opaca</i>	American Holly
<i>Ilex cassine</i>	Dahoon Holly
<i>Magnolia grandiflora</i>	Southern Magnolia
<i>Metasequoia glyptostroboides</i>	Dawn Redwood
<i>Platanus x acerifolia</i>	London Plane Tree
<i>Platanus occidentalis</i>	American Sycamore
<i>Quercus alba</i>	White Oak
<i>Quercus bicolor</i>	Swamp White Oak
<i>Quercus coccinea</i>	Scarlet Oak
<i>Quercus falcata</i>	Red Oak
<i>Quercus macrocarpa</i>	Bur Oak
<i>Quercus muehlenbergii</i>	Chinkapin Oak
<i>Quercus phellos</i>	Willow Oak
<i>Quercus virginiana</i>	Live Oak
<i>Taxodium distichum</i>	Bald Cypress
<i>Ulmus americana</i>	American Elm
<i>Ulmus parvifolia</i>	Chinese Elm
<i>Zelkova serrata</i>	Japanese Zelkova

Small to Mid-Size Trees

Acer buergerianum	Trident Maple
Acer campestre	Hedge Maple
Acer ginnala	Amur Maple
Acer palmatum	Japanese Maple
Acer rubrum	Red Maple
Amelanchier arborea	Downy Serviceberry
Amelanchier canadensis	Shadblow Serviceberry
Amelanchier laevis	Allegheny Serviceberry
Carpinus betulus	European Hornbeam
Carpinus caroliniana	American Hornbeam
Cercidiphyllum japonicum	Katsuratree
Cercis canadensis	Eastern Redbud
Chionanthus virginicus	White Fringe Tree
Cornus florida	Flowering Dogwood
Cornus kousa	Kousa Dogwood
Crataegus crus-galli	Cockspur Hawthorn
Crataegus marshallii	Parsley Hawthorn
Crataegus phaenopyrum	Washington Hawthorn
Crataegus viridis	Green Hawthorn
Gleditsia triacanthos inermis	Thornless Common Honeylocust
Halesia carolina	Carolina Silverbell
Ilex vomitoria	Treeform Yaupon Holly
Koelreuteria paniculata	Goldenrain Tree
Lagerstroemia indica	Crape Myrtle
Malus floribunda	Japanese Flowering Crabapple
Magnolia virginiana	Sweetbay Magnolia
Ostrya virginiana	American Hophornbeam
Oxydendrum arboreum	Sourwood
Prunus cerasifera	Flowering Plum
Prunus serrulata	Japanese Oriental Cherry
Prunus yedoensis	Yoshino Cherry
Pyrus calleryana	Callery Pears
Quercus palustris	Pin Oak
Quercus robur	English Oak
Sorbus alnifolia	Korean Mountain Ash
Sorbus americana	American Mountain Ash
Sorbus aucuparia	European Mountain Ash
Syringa reticulata	Japanese Lilac Tree
Tilia americana	American Linden
Tilia cordata	Littleleaf Linden
Tilia tomentosa	Silver Linden

Shrubs	
Abelia grandiflora	Glossy Abelia
Berberis thunbergii	Japanese Barberry
Buddlia davidii	Butterfly-bush
Buxus sempervirens	Boxwood
Calycanthus floridus	Sweetshrub
Chaenomeles speciosa	Flowering Quince
Clethra alnifolia	Sweetspire
Cleyera japonica	Japanese Cleyera
Cotinus coggygia	Smokebush
Cotoneaster apiculatus	Cranberry Cotoneaster
Cotoneaster lucidus	Hedge Cotoneaster
Deutzia gracilis	Slender Deutzia
Enkianthus campanulatus	Enkianthus
Euonymus alata 'compacta'	Dwarf Burning Bush
Forsythia x intermedia	Forsythia
Ilex crenata	Japanese Holly
Ilex glabra	Inkberry
Ilex x Nellie R. Stevens	Nellie R. Stevens Holly
Ilex verticillata	Winterberry
Ilex vomitoria	Yapon Holly
Juniperus sp	Junipers
Kolkwitzia amabilis	Beautybush
Mahonia bealei	Leatherleaf Mahonia
Myrica cerifera	Southern Waxmyrtle
Osmanthus heterophyllus -	Osmanthus
Philadelphus coronarius	Sweet Mockorange
Pieris japonica	Japanese Pieris
Pittosporum tobira	Pittosporum
Prunus caroliniana	Carolina Cherrylaurel
Pyracantha coccinea	Scarlet Firethorn
Raphiolepis indica	Indian Hawthorn
Rhododendron schlippenbachii	Royal Azalea
Spiraea x vanhouttei	Bridalwreath Spiraea
Syringa vulgaris	Common Lilac
Viburnum carlesii	Korean Spice Shrub
Viburnum dentatum	Arrowwood Viburnum
Viburnum nudum	Possumhaw Viburnum
Viburnum plicatum	Doublefile Viburnum
Viburnum trilobum	Highbush Cranberrybush

8.8 LANDSCAPE DESIGN

Groundcover	
Cotoneaster apiculatus	Cranberry Cotoneaster
Cotoneaster dammeri	Bearberry Cotoneaster
Cotoneaster horizontalis	Rockspray Cotoneaster
Euonymus fortunei	Creeping Euonymus
Coloratus	
Hemerocallis sp.	Daylily
Jasminum nudiflorum	Winter Jasmine
Juniperus conferta	Shore Juniper
Liriope spicata	Liriope
Vinca minor	Vinca

Grasses	
Imperata cylindrica	Japanese Bloodgrass
Muhlenbergia capillaris	Mulhy Grass
Panicum virgatum	Switchgrass
Schizachyrium scoparium	Blue Fescue

Perennials	
Aster novae-angliae	Purple Dome New England Aster
Purple Dome	
Echinacea purpurea	Purple Cone Flower
Leucanthemum superbum	Snowcap Shasta Daisy
Rudbeckia hirta	Black-eyed Susan

LANDSCAPE BUFFERS

Landscape buffers have been called out on the Land Use Plan for The Point at Harbour View to provide physical and visual separation from the surrounding roadways.

- A. Existing trees will be preserved when possible and will take precedence over new plantings.
- B. A 25' landscape buffer will be provided along College Drive and Armstead Rd. 1 canopy tree and 1 understory tree per 50' will be provided, if existing vegetation is not provided.
- C. A 35' commercial landscape buffer will be provided along Interstate 664 and the associated off ramp. A staggered row of evergreen screening trees spaced 20' on center will be provided, if existing vegetation is not provided.
- D. A 50' residential landscape buffer will be provided along Interstate 664 in Area 2. A staggered row of evergreen screening trees spaced 20' on center will be provided along with 1 canopy tree per 50' , if existing vegetation is not provided.

Loading and Trash Collection

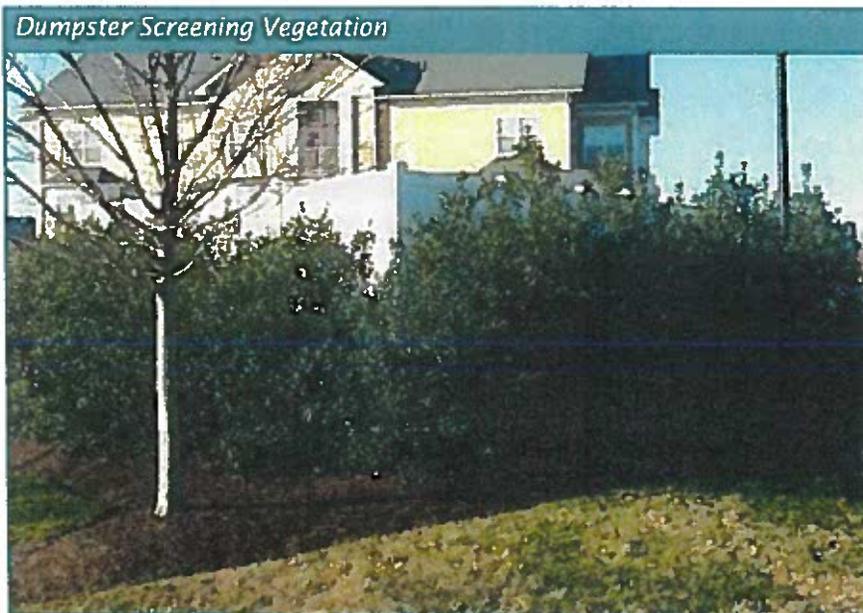
The Point at Harbour View will provide adequate loading and trash collection facilities in accordance with requirements set forth in the City of Suffolk's Unified Development Ordinance. Loading and trash areas will be accorded for at every commercial building within the The Point at Harbour View project limits. The site will follow United States Postal standards for mail collection.

A. Municipal Standards

1. Loading dimensional standards, screening, and material requirements will be per Section 31-606 (b) of the City of Suffolk's Unified Development Ordinance.

B. Non-Residential, Outparcel Loading and Trash Collection Guidelines

1. Loading and trash collection areas shall be screened from on-site and off-site views.
2. Screening walls or a vegetative screen are encouraged to screen loading and trash areas. If a screen wall is used it shall consist of the same material used on the building façade for which the loading and trash collection area is serving.
3. Vegetation may be used to soften screening walls and will be of a species providing dense cover, evergreen in its persistence, and reaching a minimum mature height of 8 feet and minimum mature spread of not less than 6 feet. Screening plant material will be placed on greater than 4 feet on-center.
4. Loading and trash collection areas shall be provided within the same screened space where ever possible.



Opaque Steel Doors



5. All loading and trash collection areas will be gated using opaque, steel material.
 6. Loading and trash collection operations or facilities will not be located within, primary building entrance aprons, walks, crosswalks, unpaved areas, or any other area not designated for the use of loading and trash collection.
 7. Number of trash collection containers will be a product of anticipated or actual volumes and privately contracted service frequency.
 8. Loading and trash collection areas will be surfaced with concrete and constructed in accordance with the City of Suffolk Public Facilities Manual.
 9. Multi-tenant buildings will maintain a shared trash collection facility.
- C. Residential Rental Trash Collection Guidelines
1. Trash facilities will use a centrally located compactor if possible.
 2. Trash collection areas will be screened from on-site and off-site views.
 3. Screening walls will consist of the same material used on the building façade for which the loading and trash collection area is serving.

9.2 LOADING AND TRASH COLLECTION

4. The capacity of compactor will be a product of anticipated or actual volumes and privately contracted service frequency.
5. As an alternative, site space may be allotted for individual dumpster areas. Should this alternative be employed the following guidelines will apply:
 - a. Trash collection areas will be screened from on-site and off-site views.
 - b. Fence material will consist of opaque, vinyl material.



- c. All loading and trash collection areas will be gated using opaque, vinyl material.
- d. Screening vegetation will be of a species providing dense cover, evergreen in its persistence, and reaching a minimum mature height of 8 feet and minimum mature spread of not less than 6 feet. Screening plant material will be spaced greater than 4 feet on-center.
- e. Trash collection operations or facilities will not be located within parking bays, parking drive isles, primary building entrance aprons, walks, crosswalks, unpaved areas, or any other area not designated for the use of trash collection.
- f. Number of trash collection containers will be a product of anticipated or actual volumes and privately contracted service frequency.
- g. Trash collection areas will be surfaced with concrete and constructed in accordance with the City of Suffolk Public Facilities Manual.

9.3 LOADING TRASH COLLECTION

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Canopies

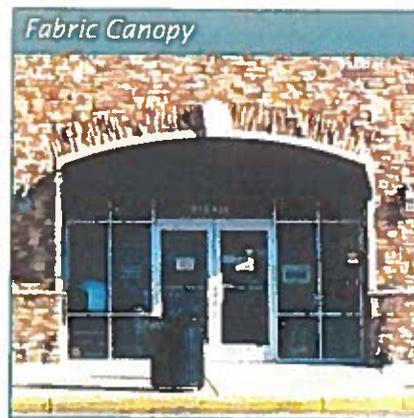
The use of canopies and awnings over pedestrian walkways are encouraged to provide shade and rain protection. Canopies and awnings also bring the added advantage of adding interest to a facade with shape and color as well as provide visual cues for various businesses and establishments.

- A. Awnings and canopies may be integrated into the façade of all commercial buildings within The Point at Harbour View and should be in character with the architectural style of the building.
- B. Awnings shall be properly placed above entrances and may extend over storefront windows. They will only be located on the first story. Awnings and canopies should be installed in fixed positions and should not be retractable.
- C. Back lit awnings and canopies, shingle and mansard canopies, and plastic awnings are prohibited.
- D. Canopies shall frame building entrances and awnings shall extend over windows. Awnings and canopies should be positioned at least 8 feet above the sidewalk. Posts that support a canopy should not interfere with the clear movement zone of the sidewalk.
- E. The color of awnings and canopies shall complement and enhance the overall color scheme of the building façade.

Canopies Should Frame Building Entrances



- F. Canopies and awnings should be made of a canvas or durable, fire-resistant fabric material that can be easily cleaned and shall include color retention properties. Hard plastic or other materials that could be cracked or broken are prohibited. They also may be made of metal and/or glass treated to withstand oxidation, corrosion, and deterioration from airborne salts.
- G. Awnings and canopies that become windblown or torn shall be repaired immediately.
- H. Awnings should maintain a consistent design and rhythm along the length of an establishment's façade.
- I. The width of an awning would typically extend to the width of a window and should not extend outward more than 4 feet from the face of the building.



Transit-Oriented Features

The Point at Harbour View will be accessed predominantly via personal automobile from College Avenue and Armstead Road. Once on site, pedestrian circulation will become the primary means of accessing various amenities and facilities. The City of Suffolk is encouraging a Focused Growth Approach in its Comprehensive Plan that will focus on development patterns that allow for additional future transportation options. Currently, Suffolk Transit provides bus service along Harbour View with connections to medical centers, shopping, and other services. A transit stop will be planned along Street Section A, dependent on uses and employment. Adequate space for transit vehicle turnarounds and bus stations should be reserved. Facilities for non-motorized transportation, including bicycles, should also be designed.

BUS ROUTES

- A. Currently one bus route serves Harbour View, the Blue route. The closest bus stop is located near Walmart on College Drive.

BUS STOPS

- A. A new city bus stop should be proposed along Street Section A or B or where uses and employment dictates
- B. The bus stops/shuttle stops should be designated with adequate signage, a noted schedule, an open fronted shelter, and appropriate seating and lighting.
- C. The bus stop will be compliant with any and all ADA accessibility requirements.



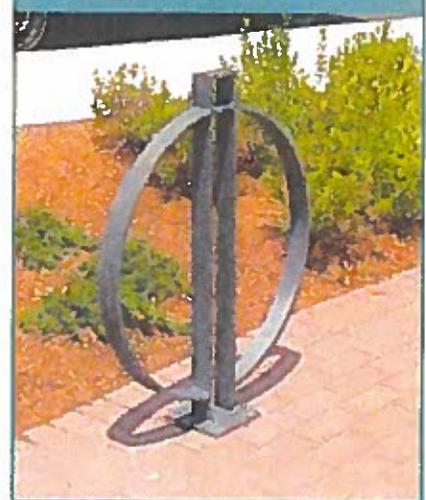
BICYCLE FACILITIES

- A. Bicycle racks may be located near the entrances of buildings, in plazas, or near parking lots. They should not obstruct pedestrian flow and movement and should be sufficiently illuminated.
- B. Bicycle racks should be of durable material and finish. Stainless steel is highly recommended as a low-maintenance option. They should be securely anchored in a concrete base.
- C. Bike lanes should be considered within the internal street network of The Point at Harbour View.

Bus Stop Signage



Bike Rack



Land Use

A. Uses permitted by right:

- Dwelling, multi-family, and apartment buildings
- Mixed-use dwelling
- Assembly, public
- Civic building
- Cultural uses, including museums and art galleries
- Entertainment, indoor
- Health club
- Hotel
- Library
- Medical clinics and dental offices
- Medical counseling centers (non-residential)
- Parks, neighborhood parks/open space
- Recreation, outdoor
- Schools, dance/art/music/business/university/trade
- Offices
- Retail sales and service
- Animal sales and care (indoor only)
- Arts, craft and boutique
- Bakery (less than 5,000 square feet)
- Book store
- Business services
- Coffee houses
- Convenience stores (non-fuel station)
- Department stores
- Fruit and vegetable stand (as part of a store front operation, outdoor display limited to 30 foot x 30 foot area or established Farmers Market)
- Hardware store (outdoor display limited to 1,000 square feet)
- Pharmacy
- Rental services, indoor display/storage
- Repair services, small appliance

- Restaurants, sit-down/delicatessen (with or without alcohol)
- Services, general
- Services, personal
- Grocery stores
- Theaters
- Motion picture and video production
- Research and development, laboratories
- Research and development, offices
- Farmers Market
- Winery or micro-brewery
- Satellite dishes (Less than 36 inches in diameter)
- Drive-through uses, as accessory to financial institution, food service, pharmacy, or clothing services
- Dry cleaning (for drop-off and pick-up only)
- Day care as part of an office use

B. Uses permitted by conditional-use permit:

- Day care (independent)
- Entertainment, outdoor
- Convenience retail center
- Bars and night clubs
- Catering, as accessory to restaurants
- Amusement center, indoor

Commensurate with the City of Suffolk's Unified Development Ordinance Section 31-701(a), accessory uses and structures should be permitted in any zoning district, but only in connection with, incidental to, and on the same lot with a principal use or structure that is lawfully permitted within such district. Accessory buildings or uses should not be constructed or established on a lot until construction of the principal building has commenced or its primary use established. Accessory buildings should not be used for dwelling purposes, except as provided in 31-701(b).

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Yards and Heights

A. Yards and Heights Table

	Minimum Front Yard	Maximum Front Yard	Minimum Side Yard	Maximum Side Yard	Minimum Rear Yard	Maximum Rear Yard	Non-Residential	Residential
Maximum Building Height	-	-	-	-	-	-	75 ft*	75 ft*
The Pointe at Harbour View Parcels adjacent proposed public right-of-ways & private streets	0 Ft	25 Ft	0 Ft	10 Ft	0 Ft	0 Ft	-	-
Outparcels adjacent College Dr. or Armistead Ave	20 Ft	50 Ft	5 Ft	10 Ft	0 Ft	0 Ft	-	-
Adjacent to I-664 & Off Ramp	-	-	-	-	25 Ft	-	-	-

Acceptable encroachments within setbacks will be commensurate with the City of Suffolk's Unified Development Ordinance Section 31-407(e)

*Prominent or signature buildings may exceed maximum building height with review and approval by Director of Planning.

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Parking

The developer will provide adequate parking to site users in accordance with parking requirements set forth in the City of Suffolk's Unified Development Ordinance. Parking areas will be accessed internally from the internal feeder road.

Two principal parking types provide for shared parking throughout the development and are:

- Surface parking, off-street
- Surface parking, on-street

A. Municipal Standards

1. Parking dimensional standards, parking rate requirements, shared parking allocation, and acceptable surfacing material will comply with Section 31-606 of the City of Suffolk's Unified Development Ordinance.

B. Parking Guidelines

These standards will focus on the parking areas that support the residential and non-residential outparcel areas.

1. Parking will be constructed in accordance with the City of Suffolk Public Facilities Manual.
2. Shared parking arrangements between uses are encouraged within this development. See item A.1.

On-Street Parallel Parking



3. Drive aisles will be two-way to ensure adequate width for entering and exiting a parking space. Stall configuration should be 45, 60, or 90 degrees throughout the site.
4. Proximity of parking areas to buildings will be per City of Suffolk's Unified Development Ordinance.
7. Dead-ended parking drive aisles should be avoided wherever possible.
8. Drive aisles and parking bays will be surfaced in accordance with the City of Suffolk Public Facilities Manual.
9. Crosswalk surfacing within parking areas will subscribe to the crosswalk standards established in the Streetscape section of these guidelines.
10. On-street parallel parking will subscribe to the City of Suffolk's Unified Development Ordinance.



14.2 PARKING

Residential Parking Shelter*Commercial Parking*

11. Appropriate knock-out spacing and rhythm should be considered to compliment aesthetic structure within parking areas. Where overhead features, building structure, or utility conflicts do not compete planting knock-outs should include a large canopy tree.
12. Large canopy and flowering trees should be included within interior parking lot green space where feasible.
13. Shrubs and groundcover planting is recommended for interior parking lot green space but turf or mulch infill is acceptable.

14.3 PARKING

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Density

The Point at Harbour View will be designed as two distinct components. The residential area must adhere to a density following a "unit per acre" calculation, and the outparcel component must comply with "floor area ratios."

A. Residential

1. A minimum density of 13 units per acre and a maximum of 40 units per acre are established in the City of Suffolk Development Ordinances.

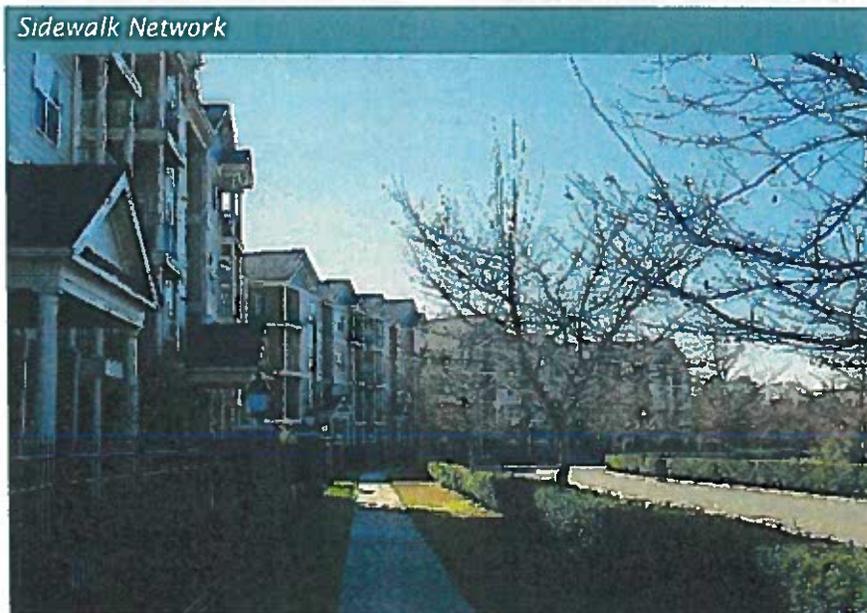
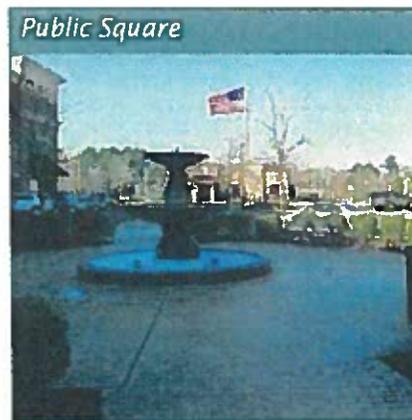
B. Outparcel

1. Commercial development will adhere to the maximum floor area ratio outlined in the City of Suffolk code.

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Open Space

The Point at Harbour View shall contain usable public open space such as plazas, centrally-located greens, public squares, outdoor dining in commercial areas, playgrounds in residential areas, and focal features. Parking lots, driveways, other vehicular areas, and buildings are not considered open space. An interconnected network of sidewalks should lead to the public open spaces from housing, parking, shopping, dining, and offices. Open space should be usable and functional, and enhance properties and their values. Land close to developed areas or adjacent to businesses should be reserved for plazas, central parks, greens, or squares.

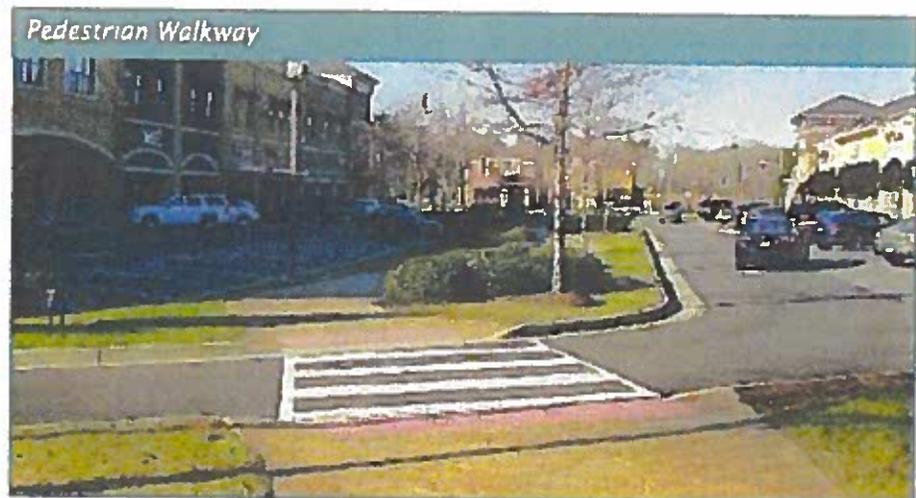


MINIMUM OPEN-SPACE RATIOS

1. Minimum open-space ratios for the multi-family residential district will provide a minimum of 100 square feet per unit of usable common open space. The open-space requirement shall be met if the development satisfies one of the following criteria:
 - a. All units are located within 1,000 feet of a public park as measured along a public sidewalk, trail, or bikeway; or
 - b. The development includes active recreation amenities for residents such as pools, tennis courts, fitness facilities, or playgrounds.

PLAZAS AND SPECIAL PAVED AREAS

- A. Pedestrian walkways for the commercial district should be per Section 31-606(a)(12) as below:
 1. Sidewalks at least 5 feet in width should be provided along all sides of the lot that abut a public street.



16.2 OPENSPACE

Active open spaces (that will include benches, sidewalks, gathering areas and pet areas) are important to the development and will be featured in both residential and commercial areas. The developer will work closely with staff during the plan submittal process to ensure high quality and appropriate locations to promote use and site capability.

2. Continuous internal pedestrian walkways, no fewer than 4 feet in width, should be provided from the public sidewalk or right-of-way to the principal customer entrance of all principal buildings on the site. At a minimum, walkways should connect focal points of pedestrian activity including transit stops, street crossings, building and store entry points, and should feature adjoining landscaped areas that include trees, shrubs, seating areas, flower beds, ground covers, or other such materials for no less than 40 percent of its length.
 3. All internal pedestrian walkways should be distinguished from driving surfaces through the use of durable, low-maintenance surface materials such as pavers, bricks, or concrete to enhance pedestrian safety and comfort as well as the attractiveness of each walkway.
- B. Outdoor plaza material and design should be coordinated with the adjacent building architecture.
- C. Plazas may be located near a main entrance or between buildings and may include amenities such as fountains, plantings, lighting, decorative paving, or seating. Amenities will not interfere with pedestrian access to building entrances, sidewalks, or parking lots.



16.3 OPENSOURCE

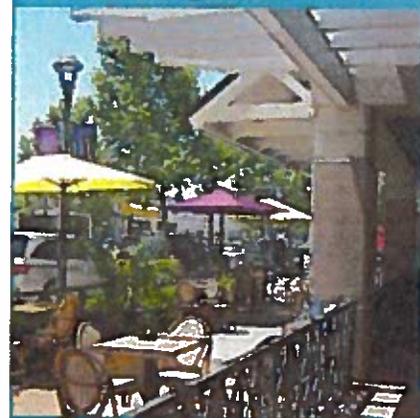
OUTDOOR DINING AND CAFES

- A. Outdoor dining areas shall be coordinated with the adjacent building architecture.
- B. Outdoor dining areas and cafes shall be positioned to allow a sidewalk area for pedestrians, be placed adjacent to the corresponding building, and shall meet all ADA standards.
- C. Durable and high-quality furnishings should be selected.
- D. Outdoor dining areas that serve alcohol must be surrounded by a barrier in all cases. One barrier access point is allowed and should be controlled by the restaurant establishment. Restaurant and eating establishment owners will maintain compliance with Virginia Alcohol and Beverage Control standards for serving alcohol outside of enclosed businesses.
- E. Generally, barriers should be 36 inches in height, and must be free-standing, stable, and removable.
- F. Fences or planters may be used as barriers. Sectional fencing should be composed of metal, wood, brick, or stone and painted or finished in the owner's choice of color to compliment the building architecture. Sectional fencing may be also be composed of aircraft cable, fabric, steel, or iron elements.
- G. Barriers should not be permanently attached to any structure or sidewalk and should be well-balanced and difficult for pedestrians to topple, trip over, or remove.
- H. Planters and the plants they contain should be well-maintained and kept free of litter and debris. Planters should contain living plants. Planters should be no more than 36 inches in height.
- I. Canopies, awnings, or table umbrellas are encouraged and may be used to provide shading and screening for the diners.

Outdoor Dining with Table



Outdoor Dining



- J. A range of furniture styles, colors, and materials are permitted, however furniture and fixtures must be clean and free of fading, corrosion, splinters, dents, tears, and chipped paint. Acceptable materials include metals, finish grade woods, and sturdy recycled materials. Chairs may incorporate fabric elements or may be upholstered but should be maintained in clean conditions. Water-resistant fabrics are strongly encouraged.
- K. Umbrellas should be suitable for outdoor patio use. Canvas is preferred. When open, umbrella canopies should be between 7 and 10 feet tall.

OUTDOOR FURNITURE

- A. Outdoor furniture shall be of a durable material and finish. All exposed metals will be coated or otherwise treated to withstand oxidation/corrosion, abrasion, and damage from airborne salts. Maintenance will be required at regular intervals. All outdoor furniture should be set level and should be securely fastened to a concrete base.
- B. Selection of outdoor furniture should be unified throughout both the commercial and residential districts in order to create a cohesive development.
- C. Benches and seating areas should be utilized in plazas, transit stops, building entrances, squares and other open-space locations. Seating areas should not obstruct pedestrian flow and movement.
- D. Benches may have intermediate armrests.
- E. Trash receptacles should be distributed throughout both the commercial and residential districts. They should be placed at corners, in plazas, and at mid-block locations along lengthy streets.
- F. Trash receptacles will be coordinated with other outdoor furniture selections. They should be securely anchored to a concrete base.

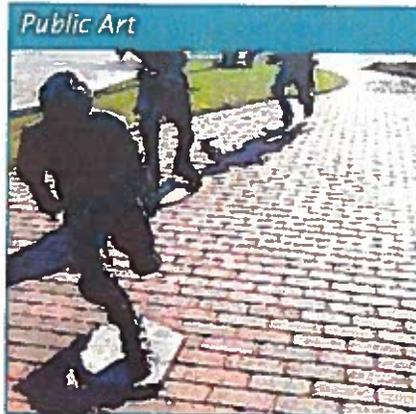


Trash Receptacle and Bike Racks*Bollards*

- G. Bicycle racks shall be of durable material and finish. Stainless steel is highly recommended as a low-maintenance option. They shall be securely anchored in a concrete base.
- H. Bicycle racks may be located near the entrances of buildings, in plazas, or near parking lots. They should not obstruct pedestrian flow and movement and shall be sufficiently illuminated.
- I. Bollards should be decorative metal, stone, or textured concrete. They may be permanent or removable and should be coordinated with building architecture and other outdoor furnishings.

PUBLIC ART

- A. Public art can include anything from traditional sculptures, paintings, and mosaics to manhole covers, paving patterns, lighting, or other elements created by an artist.
- B. Businesses can commission their own public art piece, or a committee can be established between local businesses and other representatives of the community in order to commission a public art project.

Public Art*Public Art*

Signs

SITE SIGNAGE

Signage for the The Point at Harbour View site will be clear and concise. It should lend to the character of the overall place with uniformity throughout the different development pads and a hierarchy of scale, which speaks to visitors arriving by foot or by car. The following site sign criteria identifies the signage required for development within The Point at Harbour View.

A. General Site Signage Criteria

1. Site signs shall be incorporated with the planting, paving, and lighting designs as much as practical, except for incidental signs such as temporary advertisements or sandwich board A-frame signs.
2. Light poles and building columns may be used for mounting way-finding and regulatory signs as appropriate.
3. Materials such as select grade painted wood, sheet aluminum, and graffiti and tamper-resistant materials are encouraged for site sign faces. Other highly durable sign materials such as ceramics, aluminum, steel, acrylic, or fiberglass should be considered.
4. Sign graphics should be capable of rendering high-resolution full color images, photographs, colorful diagrams, and bold text in all weather conditions. They will have UV coatings that are long lasting and resistant to graffiti, fingerprints, dirt, oil, and other substances, and they should be strong and durable and virtually unbreakable. They should comprise one solid continuous piece or several interlocking tiled pieces for larger signs.
5. Sign frames and bases should not be prone to peel, fade, crack, or rust.
6. Except as noted in this document, site signage for commercial uses should meet the minimum requirements of the Suffolk Unified Development Ordinance, Section 31-714, (L), "Signs Permitted in General Business Districts."
7. Except as noted in this document, all other site signage shall meet the minimum requirements of the Suffolk Unified Development Ordinance, Section 31-714, "Signs."

B. Main Entrance Sign

1. The main entrance to The Point at Harbour View should consist of masonry or decorative concrete structures and signage, including a focal point, base planting, and lighting.
2. Sign graphics and lettering should be adequately sized for legibility for pedestrians and vehicles traversing College Drive.

Main Entrance Sign Example

3. Signs shall be planned to a height that does not block visibility at corners of roadway intersections and limited to 24 feet in height. Signs should be sited entirely on private property.
 4. Sign lettering shall be of sufficient height so as plant materials will not obscure legibility of the lettering upon plant maturity.
 5. Maximum sign area shall be in accordance with the Unified Development Ordinance, Section 31-714, paragraph L.
 6. The sign may list The Point at Harbour View tenants.
- C. Secondary Entrance Sign
1. A secondary entrance sign structure shall match the main entrance sign in material, style, and context, except that it should remain proportionally smaller.
 2. The sign may list the The Point at Harbour View tenants.
 3. The sign will be lit and planted.
 4. Each side of the sign is limited to 48 square feet in area.
 5. The secondary entrance sign is limited to 15 feet in height.
- D. Residential Entry Signs
1. The residential entry sign shall consist of masonry wall and/or pier structures matching the material, style, and context of the main and secondary entrance signs.
 2. The residential entry sign is limited to 24 square feet in area.
 3. It is limited to 6 feet in height.
 4. The sign will be lit and planted.
- E. Pedestrian Way-Finding Signs
1. Pedestrian way-finding signs are limited to 6 square feet in area.
 2. Maximum height should not exceed 6 feet.

17.2 SIGNS



F. Directory Signs

1. A directory sign may be located on site and central to the retail development being proposed.
2. One directory sign may be located at each major intersection of the internal roads.
3. Directory signs are limited to 36 square feet in area and 6 feet in height.
4. Lighting for the signs is required.

G. Directional/Informational Signs

1. Directional/informational signs for pedestrians shall not exceed 6 square feet in area.
2. Directional/informational signs shall meet ADA requirements for design, access, and visibility.
3. Signs should be lit.

H. Distinct/Special Signage

Special Districts or areas of distinction that may warrant a special type of sign may be reviewed and approved by the Director of Planning.

- I. One large sign seen from Interstate 664 shall be allowed. Design shall be completed during final plan review. Sign shall generally comply with electronic message board requirements 31-714(L)(C). Height and design shall be reviewed and approved by the Director of Planning.

BUILDING SIGNAGE

The building signage criteria will serve to guide the overall scale, proportion, and location of signs for a variety of building and leasing scenarios fitting primarily one- and two-story retail/office structures at The Point at Harbour View.

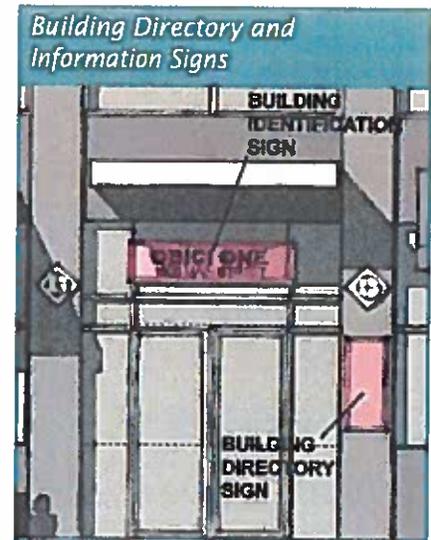
A. General Building Signage Criteria

1. Names, marks, emblems, or logos less than 4 square feet in area are not included in the calculation of any sign area.

2. First floor tenants are permitted four signs total from the following sign types: major tenant signs, storefront signs, window signs, awning signs, and blade signs.
3. Signage on a corner which establishes a visual presentation to two streets should be counted as two signs. Corner signs are only available for tenants that are leasing the corner space.
4. First floor tenants must lease 30 feet or more of continuous building frontage in order to qualify for major tenant signage.
5. Except as noted in this document, building signage for commercial uses shall meet the minimum requirements of the Suffolk Unified Development Ordinance, Section 31-714, (L), "Signs Permitted in General Business Districts."
6. Except as noted in this document, all other building signage shall meet the minimum requirements of the Suffolk Unified Development Ordinance, Section 31-714, "Signs."

B. Building Directory Sign

1. A building directory sign attaches to the face of a building and only lists the building occupants.
2. Only one building directory sign per public lobby entrance is permitted (per side).
3. No building directory sign are permitted above the first floor of any building.
4. Building directory signs are limited to 8 square feet in area.
5. Second floor tenants are permitted to have identification on building directory signs.
6. All tenant identification should be uniform and in-keeping with the general character of the building directory sign.



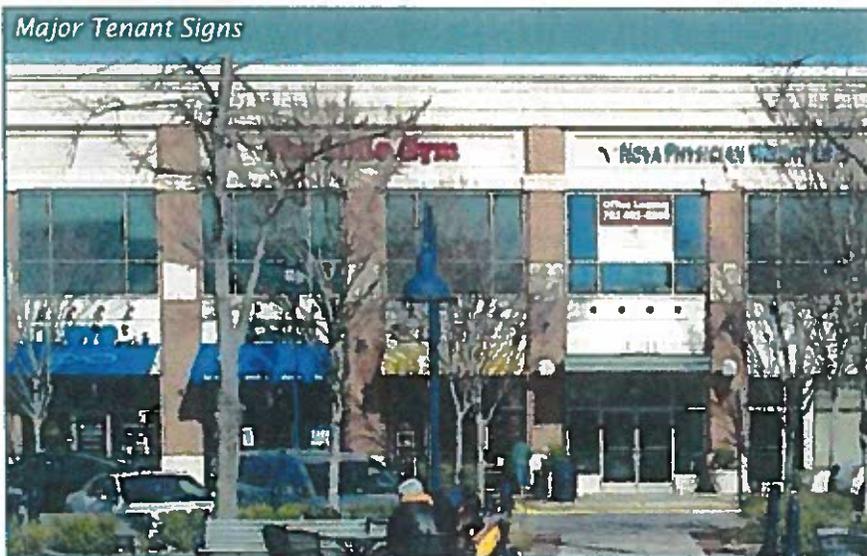
C. Building Identification Sign

1. A building identification sign is a building-attached wall sign reserved for general identification of a building, inclusive of the building name and address.
2. Only one building identification sign is permitted per lobby entrance.

3. Building identification signs are limited to 16 square feet in area.
4. Building identification signs may be located adjacent to building lobby entrances between 4 and 20 feet above grade.

D. Major Tenant Sign

1. A major tenant sign is a building-attached wall sign reserved for the principal tenant occupying the subject building.
2. Major tenant signs are limited to 4 square feet in area for every linear foot of frontage, but should not be greater than 100 square feet in area.
3. Major tenant signs are to be located below the roofline/parapet of one- and two-story buildings and generally above the space being leased.

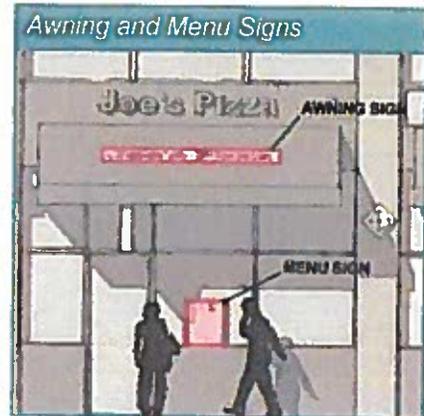


E. Storefront Signs

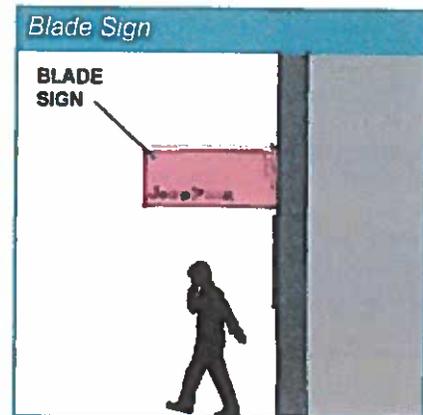
1. A storefront sign is a building-attached wall sign reserved for the tenants occupying the ground floor of the subject building.
2. Storefront signs should be located above entry doors and beneath second floor windows.
3. Only one storefront sign is permitted per tenant per primary building face.
4. Storefront signs are limited to 50 square feet in area.

F. Awning Signs

1. An awning sign is any sign mounted in any way to any kind of building awning.
2. Awning signs are limited to 12 square feet in area or 25 percent of the awning, whichever is smallest.
3. Only one name, emblem, logo, or inscription is permitted per awning.
4. Awnings will not cover any portion of upper floor windows.
5. Awning signs shall not be installed lower than 8 feet above grade.

**G. Blade Signs**

1. Blade signs are suspended signs, projected signs, marquee signs, or signs otherwise supported in a manner presenting perpendicular to the building face.
2. Blade signs are limited to 12 square feet in area.
3. Blade signs should be installed no lower than 8 feet and no higher than 14 feet above grade.
4. Blade signs shall not extend more than 5 feet from the face of the building.



Blade Signage

H. Menu Signs

1. Menu signs are similar to preview boards. They are mounted to the face of a building for pedestrians to review food menus prior to building or cafe entry. As opposed to menu boards, menu signs are neither for vehicular drive-through nor ordering food.
2. Only one menu sign is permitted per eating/drinking establishment.
3. Menu signs are limited to 4 square feet in area and should meet ADA requirements for design and access.
4. Items on menu signs should be uniform, and the menu sign should be in-keeping with the overall design of the eating/drinking establishment.

I. Window Signs

1. A window sign is any writing or imagery visible from the outside within any type of window frame and positioned anywhere within 4 feet of the window.
2. Window signs are limited to 20 square feet in area or 10 percent of the total area of the window, whichever is less.
3. Window signs may be located anywhere within the fenestration opening.

RENTAL/REAL ESTATE SIGNS

Temporary, non-illuminated signs for advertising the sale, lease, or rental of a developed or undeveloped site are generally permitted in the City's sign ordinance. Below are further criteria which apply specifically to The Point at Harbour View.

- A. Undeveloped Sites
 - 1. Only one free-standing sign is permitted on each undeveloped parcel, unless on a corner lot.
 - 2. The sign area will not exceed 12 square feet per sign.
 - 3. The sign height is limited to 6 feet above adjacent grade.
- B. Developed Sites
 - 1. Only one sign is permitted per each commercial unit to be leased, unless the unit has two entrances.
 - 2. Only two signs are permitted per each on-site residential leasing office.
 - 3. Signs should be no more than 8 square feet in area.
 - 4. Maximum height should not exceed 6 feet.

PROHIBITED SIGNS

The following signs are prohibited at The Point at Harbour View and are further defined as listed in the City Sign Ordinance, Section 31-714(c).

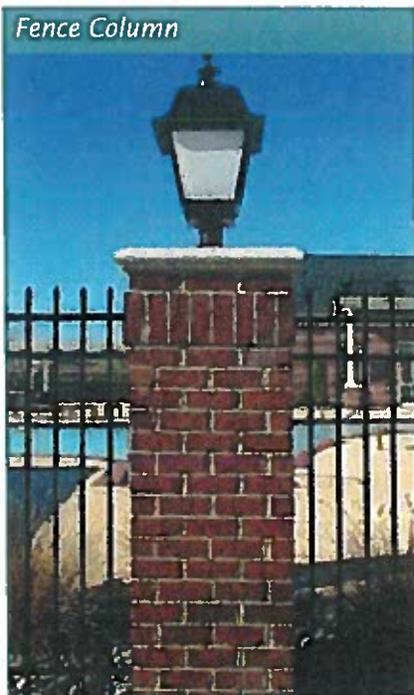
- A. Discontinued Business Signs
- B. Permanent High Intensity Signs
- C. Snipe Signs
- D. Signs Resembling Official Signs and Signals-Signs imitating or resembling official traffic or government signs or signals
- E. Signs on Vehicles
- F. Signs announcing Illegal Activities
- G. Signs Above Roof Lines
- H. Portable Signs
- I. Off-Premises Signs

Structures and Furnishings

A. Structures

Structures at The Point at Harbour View include but are not limited to fences, railings, and walls. These elements are critical for defining pedestrian spaces and providing security, comfortable grading for pavements, and safety. They define the vertical plane and allow users to gain better orientation when navigating a site. The following criteria should apply to these structures:

1. Walls shall maintain positive drainage away from their foundations, and retaining walls should have a wall drain and weeps to reduce surcharging and efflorescence.
2. Screen walls shall maintain a height equal to the element being screened up to 8 feet high.
3. Exposed metal shall be treated to withstand oxidation, corrosion, and deterioration from airborne salts in coastal environments.
4. Fencing may comprise metal, masonry, cedar wood, or an approved composite material.
5. Metal fence options may include stainless steel rod, iron picket, painted steel, or cast aluminum, all of which shall remain compatible with the surrounding materials proposed for buildings within The Point at Harbour View.



6. Metal trellises may be acceptable for decorative purposes, but shall not be used for dumpster screening.
7. Chain link fence is not permitted unless temporarily around a construction site.



B. Street Furnishings

Street furnishings are an essential component of The Point at Harbour View. Items such as benches, bike racks, trash receptacles, pet waste stations, and bollards will distinguish the human scale of the large outdoor environment. Combined with site lighting, planting, and signage, these components help to define the nature and character of the pedestrian experience while providing convenience, safety, and comfort. These details will help create a sense of place and help to establish the character of The Point at Harbour View.

1. Street lighting, benches, trash receptacles, and bike rack amenities should be coordinated within the overall plan for coherent character. Sub-areas, such as outparcels within the project site, may have slightly differing themes, but these themes should strongly resemble each other in their materials, colors, and forms throughout.
2. Benches, trash receptacles, and bike racks should be located regularly throughout the site. They should be designed for heavy use, and they should be fixed to the ground surface.
3. Iron or concrete bollards may be used to protect and define important pedestrian-oriented areas.
4. Trash receptacles should reside in an area convenient for trash pickup, but should be discrete to the view of vehicular and pedestrian traffic. They should be located at street intersections, near retail storefronts, and in community and neighborhood pocket parks. However, the receptacles should be coordinated with plant material and strategically positioned behind walks or near entrance planting.

Bench and Trash Receptacle



5. Benches should be located in pairs and should be positioned for conversation and good viewing. When on streets, benches should face the storefronts or away from the streets in more vehicle-dominated environments. They shall be located so that they do not impede pedestrian flow, and they should be coordinated with shade when exposed to southern aspects. Benches should be located on level concrete pads for comfortable seating and easy maintenance underneath them. Benches should be located at bus stops.
6. Bicycle racks should be located at multiple pocket parks and commercial areas. They should be carefully situated near the street access points but behind the entrance planting and walkways. Clusters of "Inverted U" racks or equivalent should be used.
7. Bus shelters also can comprise street furnishings and can be designed so as to infuse public art into the streetscape.

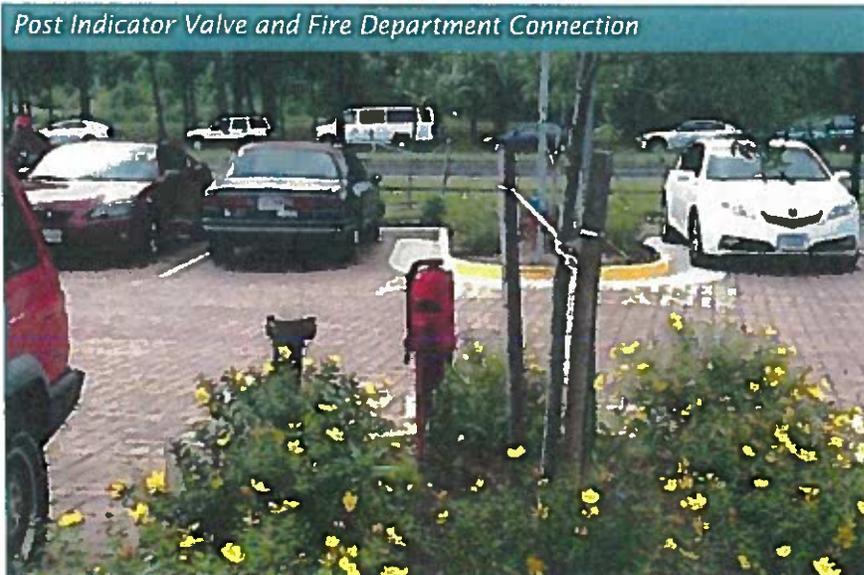
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Utilities

WATER

Water supply to the site and to each individual building is a necessity that often goes unnoticed by the general public, as long as water continues to come out of the tap. The coordination and design involved in getting adequate water flow and pressure to a large site with multiple buildings can be challenging, especially when multiple construction phases occur with build-outs and lot configurations that are only schematically planned when the water mains are installed along streets. Quantity, sizes, and locations of water meters are just some of the below ground elements that have real impacts to above ground amenities such as pavements, planting, lighting, and development footprint. Utility easements, when required, often delineate large areas that cannot have any number of these amenities encroaching into it. Other elements such as backflow preventers, hydrants, post indicator valves, and fire department connections can dictate many of the site aesthetic elements if they are not coordinated early in a building project. Below are some general guidelines for the design of on-site water systems in order to minimize conflicts and retain design intent during construction of The Point at Harbour View:

- A. Locate domestic meters, combo meters, and detector checks in the public right-of-way when practicable. When located on a private site and public easement are involved, ensure coordination of plant materials, fences, and pavements so that the easement doesn't appear as a gap in the desired design treatment.



Screened Backflow Preventer

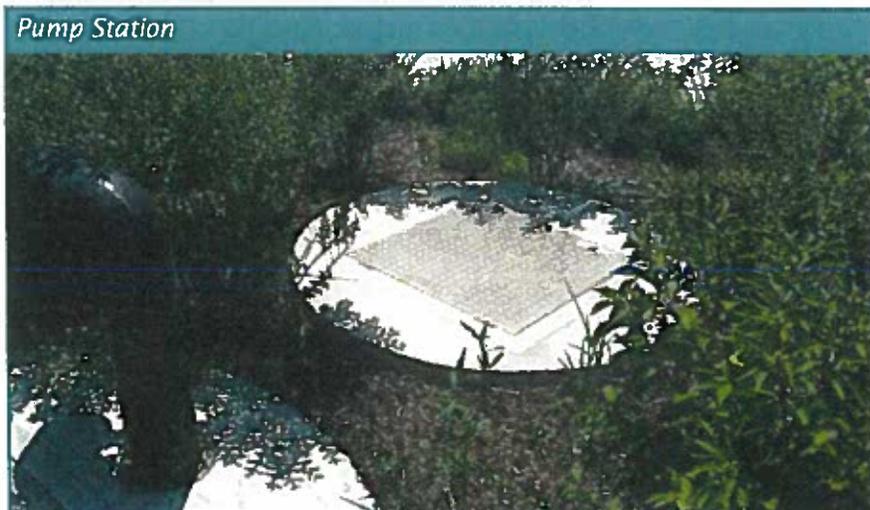
- B. Locate any backflow preventers and/or booster pumps inside the building(s) being served.
- C. Backflow preventers not able to fit in the building shall be screened from public view with plant material tall enough to hide the device or the surrounding hotbox if one is being used.
- D. Coordinate locations of hydrants and other in-line water apparatuses early in the site design in order that trees, lighting, and pavements are not in conflict with these elements.
- E. Well water for irrigation, if proposed, shall be tested by the City of Suffolk Water Quality Department for compliance with state and federal regulations. Irrigation meter or wellhead, controllers, electrical panel, and valve boxes, should be located inconspicuously.

SANITARY

Site sanitary is less flexible than water distribution since it usually relies on gravity lines which cannot bend around other conflict utilities the way a pressurized water line can. Considering the shallow depths which often limit the routing of sanitary piping and the maximum lengths required for cleanout of such pipes, manholes have a limited range for their locations. With this in mind, site elements need to be coordinated early with other elements in order to ensure that overall design efforts are not thwarted by incidental changes or oversight of these elements. Requirements for sanitary-related items on the site include the following:

19.2 UTILITIES

- A. Manhole lids shall not be located over top of sidewalks unless they can be largely centered and are decorative in nature, walkable, and ADA compliant.
- B. Private pump stations shall be screened by plant material and located away from general public view to the extent possible. Alarm indicators for pump stations should be visible, however, the structures and associative equipment should be coordinated with other site elements in order to appear better integrated with the overall site.
- C. Sanitary cleanouts shall be located uniformly along the frontage of long buildings and coordinated so that they are within planted areas if possible. Cleanout located in pavements should be coordinated so that they are flush with pavements, centered within pavement fields, and colorized to complement or match the pavement.

Decorative Manhole Cover*Pump Station*

LIGHTING

The City regulates street lighting for public rights-of-way and will require that minimum standards are met for any portions of the project site that are planned or are later determined to be public streets. It is important to recognize the differences between lighting requirements for public roadways versus private roadways and development sites. Roadway lighting for small collector streets usually requires small roadway or pedestrian lights primarily for illuminating roadways and driveways, parked cars, and pedestrian activity. These lights, often provided through Dominion Energy Virginia, consist of a high pressure sodium light source which renders a yellowish color where it illuminates. Fixtures are often limited to the Ultra style or the Colonial style with low-mounting heights for improved pedestrian visibility and scale. Without the benefit of using cutoff fixtures, shielding, or shading, it is normal for these lights to have some minimal amount of light spill over onto private lots. Roadway lighting for private streets may take on different characteristics than public street lighting. These differences are evident in terms of light source, spacing, and style, often matching or at least complementing the adjoining development lots.

Proper site lighting provides for the safety and accessibility of outdoor spaces at night. Fixtures should be selected for lighting capacity as well as architectural quality that will lend a sense of place and articulation to the site. To the maximum extent possible, light fixtures should be selected for consistency throughout the planned project area. They should be designed as cutoff fixtures to minimize light pollution and glare. They also should offer a true color rendition in lieu of yellow (high pressure sodium) street lights. Some pedestrian lights may be timer-controlled to discourage late night access to certain areas as well as to conserve energy, while other areas may use low-level pedestrian accent lighting to illuminate pathways and special pavements during nighttime activities. These could range from low-voltage lighting to solar-powered fixtures.

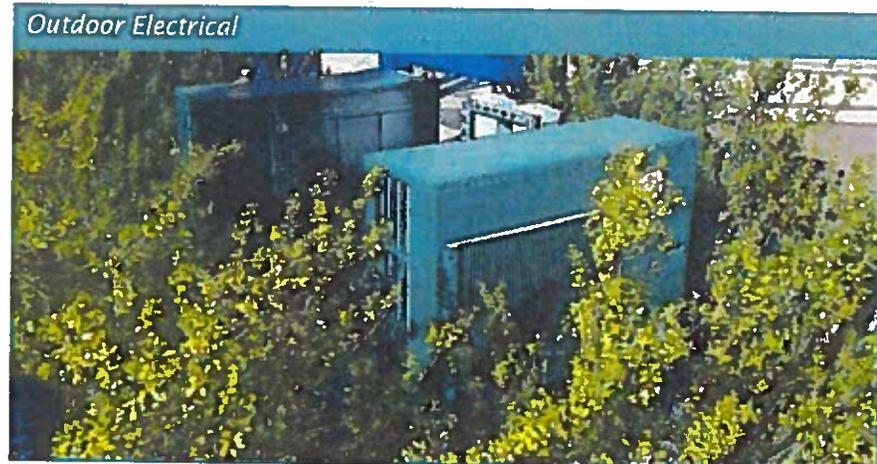
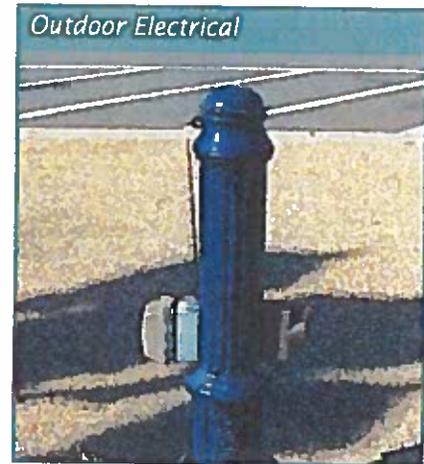
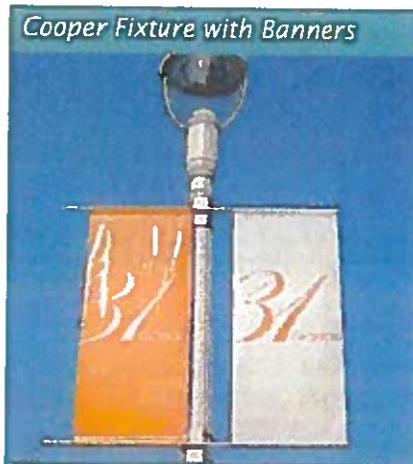
- A. All pedestrian areas and vehicular areas on site are to be lighted.
- B. Light emitting diode (LED) lighting is recommended for this site due to its low energy use, low long-term maintenance costs, and increasing availability.

19.4 UTILITIES

Bollard Lighting*Wall Mounted Lighting*

- C. Site lighting should meet the following parameters:
1. Coordinate with trees in parking lot islands so that tree branch growth will not obscure lighting at mature growth.
 2. Locate poles in parking lot islands when practicable (coordinate with trees).
 3. Locate light poles behind parking lot curbs instead of in the middle of parking stalls.
 4. Coordinate location of electrical transformers in unobtrusive areas of the site.
- D. Lighting style and color rendition should be consistent throughout The Point at Harbour View.
- E. Fixtures for street, site, and low-level pedestrian lighting should complement one another.
- F. Tool-less entry fixtures should be used for ease of maintenance.
- G. Building-mounted lighting fixtures should coordinate with street and site lighting in terms of illumination areas as well as fixture style and lamp.
- H. Lighting of building facades and signage is encouraged, provided that unwanted glare is shielded from neighboring businesses and homes.
- I. Light poles should be direct-bury fiberglass or better material.
- J. Pole-mounted lighting for pedestrians should range between 12 feet and 16 feet abovegrade.
- K. Light poles may be used for regulatory or wayfinding signs provided they are minimized and mounted cleanly.

- L. Banners may be located on light poles if bracket arms are cleanly attached.
- M. Pedestrian lighting also may include bollard lighting, recessed in-ground fixtures, low-level accent lighting, and wall mounted lighting provided character of the fixtures complement the overall site aesthetic and are coordinated with other light fixtures on the site.



Dominion Energy Virginia (DEV) has supplied the Cooper-type fixture for pole-mounted lighting styles on various other municipal site and roadway developments. This is one of the more modern lights offered by DEV. This fixture would lend itself well to The Point at Harbour View since it resembles the classic acorn-shaped Colonial fixtures. Any similar light fixture recommended would be well suited to reflect this style; however, selections are often limited to DEV standards. Special lighting fixtures that deviate from their inventory may require a special lighting agreement with DEV if they are to maintain these lights.

OTHER ELECTRICAL UTILITIES

Other than lighting, there are other site electrical utilities that support building and site development which merit attention and care in order to prevent conflicts with other site elements. These elements include generators; transformers for lighting and for buildings, meters, junction boxes, underground conduits; and switches. General considerations should be made for these items which include the following parameters:

- A. Generators will be screened substantially with a wall or fence structure to reduce noise and block general site lines. Plant material should be used to soften the screening structure surrounding the generator. Any screening will need to account for minimal air circulation and safety clearance requirements. Screen walls or fence material should be integrated into the building and should generally match in materials and color.
- B. Transformers should be planned and located early to coordinate with plant materials, sidewalks, curbs, and other site amenities in order that these items are not left to resolve during construction. Transformers should be screened with plant material, and screening should allow room for minimal clearances mandated by the power company.
- C. Meters should be screened from public view and should be integrated with the buildings.
- D. Junction boxes and related electrical switches and wiring boxes and devices should be screened from view with plant material and they should be located off of pavements.
- E. Conduits will be located behind walks for ease of access and maintenance. They should be sleeved under all pavement crossings.
- F. Power pedestals should be located inconspicuously where outdoor power may be required.

OTHER SITE-RELATED UTILITIES

Other private franchise utilities may include gas, fiber optic, phone, cable television, and other communications lines. They should be planned to coordinate with electrical and other underground and above ground utilities and site amenities. Criteria for these elements are as follows:

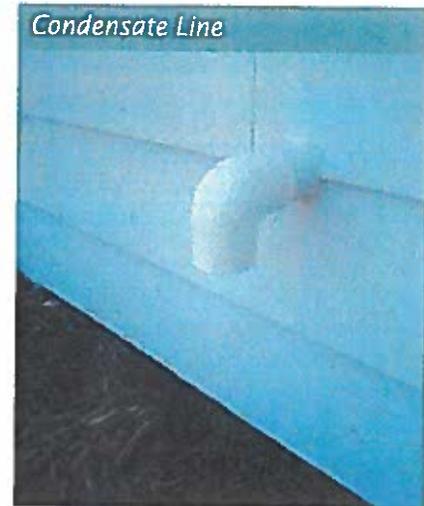
- A. Gas meters should be screened from view.
- B. All communication, information cables and lines, and their above-ground pedestals should be planned and located in advance of construction and screened with plant material.

OTHER BUILDING-RELATED UTILITIES

Other building-related utility elements may include heat pumps, chillers, and related building systems that exit the building envelope to the site.

These elements should meet the following requirements:

- A. Heating ventilation and air conditions (HVAC) units shall be located on roofs or behind the building fronts and depending on location screened with plant material and/or decorative barriers or fencing to match building materials and color. Air circulation clearances shall be maintained, and areas in between units shall be on a maintainable surface such as concrete or gravel.
- B. Condensate drain lines exiting buildings from condensate buildup in air handlers shall be coordinated with plant material and drainage so that plant beds are not drowned out and runoff does not drain across pavements.



Grading, Drainage, and Stormwater Management

Stormwater management for The Point at Harbour View should incorporate public open spaces and focal elements into the overall design of the pond and its surrounding spaces.

Due to the intended nature of the The Point at Harbour View development, new impervious surfaces will create a need for a comprehensive stormwater management strategy. This development will be designated as an Intensely Developed Area (IDA). Sheet flow from roof structures and surface parking lots will create the majority of stormwater runoff. Runoff will be collected and guided via City-standard curb and gutter, until it encounters a curb cut or drain inlet. The design of an adequate storm system should be coordinated during design development to ensure a cohesive stormwater management strategy. Practices such as installing permeable pavers or utilizing bio-retention beds should be considered to reduce the overall site runoff.

Natural Drainage



Permeable Pavers

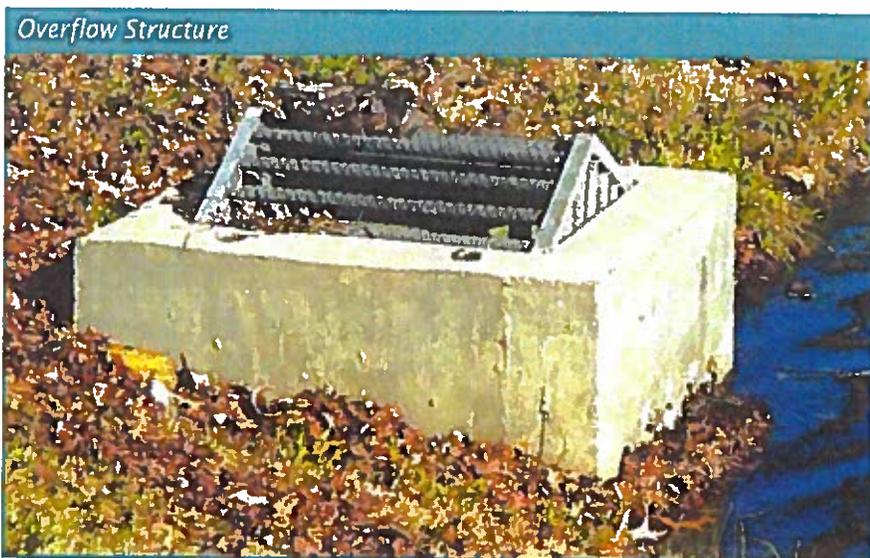


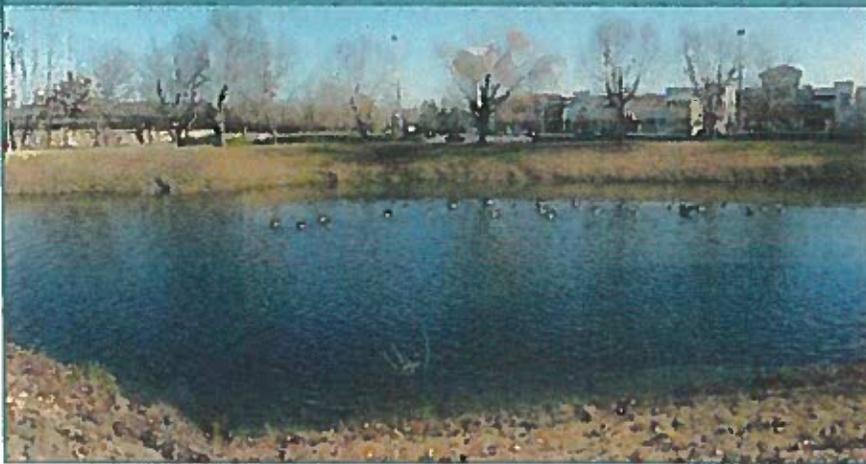
A. Strategy

Stormwater management areas will collect the stormwater from The Point at Harbour View site via underground storm pipes as well as limited surface runoff from the immediate surrounding areas. Any wet ponds should be designed as a passive recreational amenity serving as both stormwater mitigation and open space for the nearby residents. Features to be incorporated into the design of the wet pond are lighting, an aerator fountain, native plantings, pathways, and seating areas for users to enjoy.

B. Standards

1. Stormwater ponds shall reside under private ownership and maintenance.
2. A 10-foot wide aquatic/safety bench should be provided in the pond.
3. A 10-foot wide safety/access bench shall be provided. Access shall be provided to the overflow structure.
4. Basin side slopes should have 3:1 slopes or lower.
5. Fencing is not required.
6. All buildings may be located up to 25 feet from the 100-year high-water elevation of the pond.
7. Decks, patios, walkways, and roadways may be located closer than 25 feet provided that appropriate safety measures are incorporated.
8. Vegetative perimeter yards or barriers and buffer yards should be provided where open space areas are adjacent to the pond.



Wet Pond

9. Where planting is provided, the plantings should be complementary to the other planting areas throughout The Point at Harbour View providing a pleasant environment for the use of residents and visitors to enjoy the open air and water views during, perhaps, a weekend family picnic.
10. All stormwater management facilities must be designed in accordance with Virginia Stormwater BMP Clearinghouse Design Specifications.

C. Alternative BMP Designs and Innovations

1. The developer may propose alternative BMP designs and innovations that will accomplish the same water quality and quantity benefits as any proposed wet pond if the proposed design meets the following criteria:
 - a. The design solution is regional and comprehensive in nature.
 - b. The design solution is allowed under the then current versions of the referenced regulatory documents in this section.
 - c. The design solution adequately addresses replacing the aesthetic value of the pond being displaced through equal or better provision of open spaces and amenities.

The following documents are incorporated into this document by reference:

1. Virginia Stormwater Management Handbook; most current edition
2. City of Suffolk Unified Development Ordinance Article 6, Section 31-611.

Appendix A

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